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1/31/2022

Kristen Motel  
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**VIA ELECTRONIC and OVERNIGHT MAIL**

Chairman William D. Taylor  
and Members of the Zoning Commission  
Town of New Milford  
10 Main Street  
New Milford, Connecticut 06776

Re: Volta Electric Vehicle Charging/Display Kiosks  
Zoning Text Amendment Application  
Premises: Stop & Shop, 180 Danbury Road, New Milford, CT 06776

Dear Chairman Taylor and Members of the Zoning Commission:

This letter and enclosed materials are respectfully submitted on behalf of Volta Charging, LLC (“Volta”), in support of its Petition to amend the Town of New Milford’s Zoning Regulations (“Zoning Regulations”) in connection with the above captioned parcel.

Volta respectfully requests text amendments to Zoning Regulation Sections 145-020, 145-040, 145-050 and 145-070, as well as the addition of Section 145-110 to permit accessory electric vehicle charging kiosks equipped with electronic displays in business and industrial zones along the Route 7/Route 202 corridor.

In support of this application, enclosed please find eight (8) copies of this letter and the following materials:

- Exhibit A:** Application to Amend the Zoning Regulations and Letter of Authorization;
- Exhibit B:** Written Statement/Petition in support of the Zoning Text Amendment;
- Exhibit C:** Proposed Zoning Text Amendments; and
- Exhibit D:** Site Plans prepared by Kimley-Horn, P.C. dated April 6, 2021, last revised July 23, 2021.

Also enclosed is a check made payable to the Town of New Milford in the amount of \$ 360.00 representing the filing fee for the zoning amendment fee.



We respectfully request that this matter be placed on the Zoning Commission's next available meeting agenda for discussion. In the interim, should you or Town Staff have any questions, please feel free to contact the undersigned. Thank you in advance for your consideration in this regard.

Sincerely,

A handwritten signature in black ink, appearing to read "Kristen Motel", is written over a light blue horizontal line.

Kristen Motel

Enclosures

Cc: Laura Regan, New Milford Land Use Supervisor / Zoning Enforcement Officer  
Volta Charging, LLC  
Kimley-Horn, P.C.  
Christopher B. Fisher, Esq.

# **EXHIBIT A**



# Petition to Amend the Zoning Regulations and/or Zoning Map Town of New Milford Zoning Commission

Rev. 10/13/17

### TYPE OF APPLICATION

- REGULATION AMENDMENT – CHANGE OF TEXT
- MAP AMENDMENT – CHANGE OF ZONE

### FOR OFFICE USE ONLY

DATE SUBMITTED: \_\_\_\_\_  
 FEE PAID: \$ \_\_\_\_\_ CK# \_\_\_\_\_

### PETITIONER/APPLICANT

Name: Volta Charging, LLC

Mailing Address: 155 De Haro Street

City/State/Zip Code: San Fransico, CA

Phone: (917) 903-6066 Email: samuel.lee@voltacharging.com

Authorized Agent (If Different): Kristen Motel, Cuddy & Feder LLP (Attorneys for the Applicant)

Mailing Address: 445 Hamiton Ave, 14th Floor, White Plains, NY 10601

Phone: (914) 761-1300 Email: kmotel@cuddyfeder.com

### REGULATION AMENDMENT – CHANGE OF TEXT

Section of the Regulations to be amended: Sections 145-020; 145-040; 145-050; 145-070; 145-110

In addition to this completed application form and required fees, as set forth in Appendix B of the New Milford Zoning Regulations, the following must be submitted with a request for a change of text:

- Written statement signed by the applicant detailing why the text change is being proposed
- Eight (8) copies of the proposed text change; underline new language and [~~bracket and strike out language to be removed~~].
- Agent Letter of Authorization signed by the owner of record. Owner of record's original signature required. Faxed or scanned copies will not be accepted. (*if applicable*)

### MAP AMENDMENT – CHANGE OF ZONE

Location: N/A - Text Amendment Tax Map(s): \_\_\_\_\_

Property Owner: \_\_\_\_\_ Tax Lot(s): \_\_\_\_\_

Mailing Address: \_\_\_\_\_

Phone: \_\_\_\_\_ Email: \_\_\_\_\_

Current Zone: \_\_\_\_\_ Proposed Zone: \_\_\_\_\_

Reason for the proposed change in zone: \_\_\_\_\_



In addition to this completed application form and required fees, as set forth in Appendix B of the New Milford Zoning Regulations, the following must be submitted with a request for a change of zone:

- Eight (8) copies of an A-2 survey depicting the boundaries of the proposed zone change with a listing of the property owners of record within a 500 foot radius of the subject property. (*Refer to Section 200-020 of the New Milford Zoning Regulations for additional information. Maps must be folded, not rolled.*)

**One (1) copy of each of the following:**

- Written statement signed by the applicant detailing the requested change
- Name(s) and address(s) of owners of record, as of the date of application submission, of all abutting property owners and those directly across the street from the subject property (*obtain from the Tax Assessor's office*)
- Schedule A – Legal Description of the property(s) (*obtain from the Town Clerk's office*)
- Agent Letter of Authorization signed by the owner of record. Owner of record's original signature required. Faxed or scanned copies will not be accepted (*if applicable*)

*Please see section 200-030 of the Zoning Regulations for Public Hearing Notice Requirements.*

**SCHEDULE OF FEES**

*Checks are made payable to the Town of New Milford*

Regulation Amendment	\$300.00	\$ <u>300.00</u>
	\$ 60.00 State of CT Land Use Fee	\$ <u>60.00</u>
Map Amendment	\$500.00	\$ _____
	\$ 60.00 State of CT Land Use Fee	\$ _____
	Total fee submitted	\$ <u>360.00</u>

The fees set forth above are the minimum fees required and are not refundable or transferable.

**SIGNATURES (MUST BE ORIGINAL INK, NOT SCANNED OR FAXED)**

The applicant understands that this application is considered complete only when all information and documents required have been submitted and the fee paid. The applicant or their agent must be present for the meeting at which their application will be heard.

Samuel Lee  
Applicant Signature

1/24/22  
Date

Samuel Lee  
Printed Name

[Signature]  
Owner of Record Signature (*If Different From Applicant*)

1.19.22  
Date

Stephan Rapaglia, SVP  
Printed Name

\*\*\*\*\*


**LETTER OF AUTHORIZATION**

This Letter of Authorization, dated this 19<sup>th</sup> day of January, 2022, provides written authorization for Volta Charging, LLC and its affiliates, its agents or representatives (collectively, "Volta"), to apply for and execute any necessary Town of New Milford petitions, permits or any other approvals, including, but not limited to, the filing of applications for zoning amendments, site plan and special exception approvals, solely to the extent necessary for purposes of installing, operating and maintaining electric vehicle charging/display kiosks on a portion of the real property having an address of 164 Danbury Road, New Milford, Connecticut (Map/Block/Lot: 18/1/29) and owned by UB Litchfield, LLC ("Owner"). Volta will be responsible for all fees incurred in securing any required approvals and for the satisfaction of all conditions or modifications. This authorization may be revoked by Owner upon notice given to the municipality.

A copy of this letter shall be regarded as having the same effect as the original.

OWNER: UB Litchfield, LLC

By: Urstadt Biddle Properties Inc., sole  
member

By:   
Stephan Rapaglia, SVP

# **EXHIBIT B**

ZONING COMMISSION: TOWN OF NEW MILFORD  
COUNTY OF LITCHFIELD: STATE OF CONNECTICUT

-----X  
In the Matter of the Application of

**Volta Charging, LLC**

**PETITION**

Petition to the Amend the Zoning Regulations of the Town of New Milford to Permit Electric Vehicle Charging/Display Kiosks on Parcels in Business and Industrial Zones that Will Apply to Real Property Located at 180 Danbury Road, Designated on the Tax Map of the Town of New Milford as Section 18, Block 1, Lot 29 (also known as “164 Danbury Road”)

-----X  
TO THE HONORABLE CHAIRMAN AND MEMBERS OF THE ZONING COMMISSION OF THE TOWN OF NEW MILFORD

The Petition of Volta Charging, LLC (“Volta” or the “Petitioner”) respectfully shows and alleges in support of Petitioner’s request:

1. PETITIONER: Volta Charging, LLC is a corporation organized and existing under the laws of the State of Delaware and is requesting text amendments to the Zoning Regulations of the Town of New Milford, Connecticut to permit EV Charging/Display Kiosks on parcels classified in Commercial and Industrial Districts subject to specific bulk, design and visibility requirements.

Volta operates electric vehicle (“EV”) charging networks throughout the United States and provides free charging in convenient locations. By generating revenue through sponsored content, Volta creates value and passes that on to EV owners who can charge while they shop. This encourages consumer adoption of electric vehicles with net reductions in carbon emissions.

Volta is partnering with Stop & Shops across the Northeast to establish on-site electric vehicle charging kiosks that enhance the shift from combustion-powered miles to electric miles. The charging kiosks include a digital display feature for various sponsored content, which enables Volta to provide the vehicle charge at no cost to the driver while it is parked on site.

2. THE PARCELS & FUTURE PROPOSAL: The Petitioner is proposing to install two (2) EV Charging/Display Kiosks in the parking lot of the Stop & Shop located at 180 Danbury Road (also known as 164 Danbury Road) (the “Parcel”) in the Town of New Milford, Connecticut.

The Parcel is currently classified within the Industrial (“I”) Zoning District and is situated in the New Milford Shopping Plaza, which tenants include numerous retail stores, including a Stop & Shop and a Walmart, personal service shops, restaurants and a gas station.

Volta proposes to convert two (2) standard parking spaces into two (2) electric vehicle spaces with charging kiosks located in existing landscaped islands. The kiosks are

approximately seven (7) feet tall and three (3) feet wide and include digital displays that are four (4) feet tall and 2¼ feet wide.

The EV Charging/Display Kiosks will be located in the first row of parking spaces, closest to the Stop & Shop building and its entrance to target pedestrian traffic and are not intended for any off-site visibility. The digital displays themselves have an auto-dimming feature and are equipped to display subtle motion with sponsored content changing at a maximum of 8-second intervals. Concept drawings and renderings of the proposed EV Charging/Display Kiosks are annexed hereto as **Exhibit D**.

3. IN FURTHERANCE OF THE TOWN OF NEW MILFORD'S PLAN OF CONSERVATION AND DEVELOPMENT, THE PROPOSED EV CHARGING/DISPLAY KIOSKS WILL EXPAND TRANSPORTATION INFRASTRUCTURE, PROMOTE SUSTAINABILITY AND PROVIDE CONVENIENT AMENITIES: The proposed zoning amendments will permit EV Charging/Display Kiosks in limited locations to serve and complement the business and industrial zones while also enhancing the Route 7 South/Route 202 corridor. See **Exhibit C** – Zoning Text Amendments.

The Town of New Milford 2021 Plan of Conservation and Development (“POCD”) encourages the expansion of transportation infrastructure for all users<sup>1</sup> and promotes incorporating sustainability into Town regulations and development policies.<sup>2</sup> The POCD specifically identifies the task of creating “consistent infrastructure for sustainability.”<sup>3</sup> In fact, the POCD notes that “[a]dequate electrical power is expected to be available to meet the needs of local users and accommodate new technologies and uses such as electric vehicles and electric charging stations.”<sup>4</sup>

Among the POCD’s goals is to advocate for an ongoing certification in Sustainable Connecticut and create consistent infrastructure for sustainability.<sup>5</sup> As a silver-certified member of Sustainable Connecticut, New Milford strives to implement best practice “actions”, one of which is supporting zero emission vehicles deployment by creating EV infrastructure for residents and travelers.<sup>6</sup>

In encouraging New Milford to continue its sustainability efforts<sup>7</sup>, the POCD recognizes that modifications to the current zoning regulations are necessary to attract new investments<sup>8</sup> and that land use policies and regulations should reflect the type of business development envisioned for the area.<sup>9</sup> The POCD also highlights the need to improve business and industrial locations along the Route 7 South/Route 202 corridor, support development in all business and industrial zones and capitalize on business opportunities.<sup>10</sup>

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<sup>1</sup> See POCD, pg. 6.

<sup>2</sup> See POCD, pg. 139.

<sup>3</sup> See POCD, pg. 139.

<sup>4</sup> See POCD, pg. 103 (emphasis added).

<sup>5</sup> See POCD, pg. 139.

<sup>6</sup> See Sustainable Connecticut, Action 6.4 – Support Zero Emission Vehicle Deployment, <https://sustainablect.org/actions-certifications/actions/#open/action/42>.

<sup>7</sup> See POCD, pg. 122.

<sup>8</sup> See POCD, pg. 139.

<sup>9</sup> See POCD, pg. 140.

<sup>10</sup> See POCD, pg. 134.

The requested zoning text amendments are integral to encouraging sustainable infrastructure options for New Milford's citizens and visitors that also promote business development. Volta's uniquely situated EV Charging Kiosks encourage the use of electric vehicles by conveniently providing free charging stations to Stop & Shop customers. Permitting the display component of the Kiosks will enhance local economic development by enticing resident EV drivers to shop locally and enjoy free vehicle charging.

Further, this text amendment is timely given recent federal and state initiatives to encourage EV deployment and meet the future infrastructure needs of New Milford.<sup>11</sup> The proposed amendments will accommodate much needed no-cost electric vehicle infrastructure that is easily accessible from a heavily traveled State Route.

The proposed zoning amendments include criterion specifically designed to ensure that EV Charging/Display Kiosks will be compatible with surrounding land uses in this commercial district along Danbury Road. Amending the Zoning Regulations to permit EV Charging/Display Kiosks in business and industrial zones subject to specific location, bulk, design and visibility requirements will further New Milford's goals of enhancing sustainable transportation infrastructure, improving industrial and business zones, and adopting regulations that promote sustainability and attract new investment.

WHEREFORE, the Petitioner respectfully requests that this Petition be granted in its entirety thereby amending the Zoning Regulations of the Town of New Milford to permit EV Charging/Display Kiosks in business and industrial zones on parcels fronting on Route 7 / Route 202, subject to specific location, bulk, design and visibility requirements.

Dated: January 31, 2022

Respectfully submitted,



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Kristen Motel  
**Cuddy & Feder LLP**  
Attorneys for Petitioners  
445 Hamilton Avenue, 14<sup>th</sup> Floor  
White Plains, New York 10601

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<sup>11</sup> On August 5, 2021, the President issued an Executive Order establishing a goal for 50% of all vehicles sold in 2030 to be zero emission vehicles. [Executive Order on Strengthening American Leadership in Clean Cars and Trucks](#). The Connecticut Governor's Council on Climate Change (GC3) released a report in January 2021, [Taking Action on Climate Change and Building a More Resilient Connecticut for All](#), which includes the following recommendation for municipalities, in relevant part: "Expand electric vehicle (EV) charging network to ensure consumer confidence and reduce range anxiety." Pg. 38. As revealed by the State's [2021 Greenhouse Gas Inventory](#), Connecticut is currently not on track to meet its 2030 Global Warming Solutions Act (GWSA) emission target, due in part to substandard EV adoption rates. Indeed, "approximately 17,217 passenger EVs are registered in Connecticut – a small fraction of the 500,000 electric light-duty vehicles that the GC3 has projected the state would need in order to meet the 2030 GWSA targets. [2021 Greenhouse Gas Inventory](#), pg. 4.

# **EXHIBIT C**

## Exhibit C

### Town of New Milford Proposed Zoning Text Amendment

#### **Chapter 145: Signs**

##### **Section 145-020 Definitions**

Electric Vehicle (“EV”) Charging/Display Kiosk: A combined electric vehicle charging kiosk with internally illuminated LED displays.

Off-site sign: A sign located on a parcel of land which directs the public to a business or public attraction/location that is located on another parcel of land for the purpose of safety and convenience. An EV Charging/Display Kiosk is not included in this definition.

##### **Section 145-040 Prohibited Signs**

The following signs are prohibited in all zones:

1. Flashing signs, with the exception of permitted EV Charging/Display Kiosks under Section 145-110.
6. Signs with electronic displays, image displays (LED, LCD etc) or moving, scrolling, and/or continuous strip lights with the exception of permitted gasoline price signs under Section 145-020[.] and permitted EV Charging/Display Kiosks under Section 145-110.

##### **Section 145-050 Permitted Signs**

All signage described in this section requires a zoning permit.

2. Business, Industrial, Airport, and Motor Vehicle Junkyard Zones: In a B-1, B-2, B-3, B-4, I, IC, RI, Airport and MV zones, and any future non-residential zones which may be established, the following signs are permitted, as follows:
  - b. Freestanding signs, provided that only one (1) free-standing sign is allowed per lot. Freestanding sign exceptions: properties where gasoline is legally sold (not to exceed 24 square feet); properties approved to host an off-site sign in accordance with section 145-090; freestanding signs allowed as temporary under section 145-100[.]; and EV Charging/Display Kiosks allowed under section 145-110.
  - i. The maximum allowable area of a freestanding sign shall be determined as follows: the length of the lot frontage multiplied by 0.2 equals the maximum square footage allowable for the surface area of a free standing sign, up to a maximum size of forty (40) square feet in area, with the exception that in the B-4 zone, free standing signs may not exceed sixteen (16) square feet in area. Refer to sections 145-090 (Off-Site Signs) and 145-100 (Temporary Advertising Signs) for additional standards. EV Charging/Display Kiosks shall not count towards the maximum allowable freestanding sign area.



## **Section 145-070 Additional Standards**

5. Signs may advertise, identify or give publicity or notice only with respect to a use of land, buildings or structures located on the lot where the sign is located with the exception of off-site signs permitted in accordance with Section 145-090 of these regulations, EV Charging/Display Kiosks permitted in accordance with Section 145-110 of these regulations, and applicable exempt signs in accordance with Section 145-030(4), (7) and (14).

## **Section 145-110 EV Charging/Display Kiosks**

EV Charging/Display Kiosks are allowed for permitted businesses in any business or industrial zone with frontage on Route 7 and Route 202, subject to the following conditions:

1. There shall be a maximum of two (2) EV Charging/Display Kiosks per property.
2. EV Charging/Display Kiosks shall not exceed 22.5 square feet in size and 7.5 feet in height.
3. EV Charging/ Display Kiosks display screen shall not exceed 9 square feet in size.
4. EV Charging/Display Kiosks shall be equipped with an auto-dimming feature.
5. EV Charging/Display Kiosks shall limit content refresh rates to no more than every eight (8) seconds.
6. EV Charging/Display Kiosks shall be located within 100 feet of the front façade of the business building on the site.
7. EV Charging/ Display Kiosks shall be setback a minimum of 70 feet from a public right of way.

# **EXHIBIT D**



# volta

## STOP AND SHOP #683

180 DANBURY ROAD  
NEW MILFORD, CT 06776  
LITCHFIELD COUNTY

**volta**  
155 DE HARO STREET  
SAN FRANCISCO, CA 94103

**Kimley»Horn**  
1 NORTH LEXINGTON AVENUE, SUITE 505  
WHITE PLAINS, NY 10601  
Main: 914.368.9200 | www.kimley-horn.com  
© 2021 Kimley-Horn and Associates, Inc.

ITEM	TASK	YES	NO	N/A
1	CONTACT 811 UTILITY PRIOR TO EXCAVATION WORK.			
2	NOTIFY VOLTA & KIMLEY-HORN OF ANY DISCREPANCIES W/ PLANS OR POTENTIAL CONFLICTS.			
3	VERIFY ALL FIELD CONDITIONS PRIOR TO START OF CONSTRUCTION IN ACCORDANCE WITH THESE PLANS.			
4	INSTALL WORK AREA PROTECTION MEASURES.			
5	FIELD LOCATE EXISTING UTILITIES AND CROSSINGS & VERIFY NO CONFLICTS W/PROPOSED INFRASTRUCTURE.			
6	FIELD VERIFY ALL STALL DIMENSIONS AND EQUIPMENT LOCATIONS.			
7	CONFIRM ALL ADA AND LOCAL REQUIREMENTS ARE MET.			
8	ESTABLISH TEMPORARY CONSTRUCTION ACCESS(ES).			
9	IMPLEMENT AND MAINTAIN EPSC CONTROL MEASURES PER LOCAL REQUIREMENTS.			
10	LOCATE VERTICAL AND HORIZONTAL UTILITIES PRIOR TO BORING.			
11	PROVIDE PROPOSED LIMITS OF ASPHALT OVERLAY SKETCH TO KIMLEY-HORN & VOLTA (IF NEEDED).			
12	SEED & STABILIZE ALL DISTURBED AREAS AFTER FINAL GRADING.			

ALL WORK AND MATERIALS SHALL BE PERFORMED AND INSTALLED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE BUILDING/ DWELLING, STRUCTURAL, PLUMBING, MECHANICAL, ELECTRICAL, AND FIRE/LIFE SAFETY CODES AS ADOPTED BY THE LOCAL GOVERNING AUTHORITIES. NOTHING IN THESE PLANS IS TO BE CONSTRUCTED TO PERMIT WORK NOT CONFORMING TO THE LOCAL GOVERNING AUTHORITIES CODES.

**VOLTA PROPOSES:**

- APPLICATION FOR A ZONING PERMIT AND BUILDING PERMIT IS MADE FOR 2 ELECTRIC VEHICLE (EV) CHARGING STATION FIXTURE(S) TO BE LOCATED IN EXISTING CURBED ISLAND AREA(S) THAT ARE ADJACENT TO ON-SITE PARKING SPACES AND PART OF AN EXISTING STOP & SHOP GROCERY STORE AT THE PROPERTY. THE EV FIXTURE(S) ARE CUSTOMARY ACCESSORY AND INCIDENTAL TO THE EXISTING COMMERCIAL USE AND SOLELY FOR THE BENEFIT OF CUSTOMERS VISITING THE STORE. THE FIXTURE(S) ARE LOCATED TO PROVIDE PRIORITY PARKING FOR PATRONS WITH EVS AND DISPLAY VISIBILITY ALONG THE INTERIOR CIRCULATION AISLE FOR SHOPPERS. THERE ARE NO PROPOSED CHANGES TO THE PARKING SPACES OR ANY OF THE EXISTING TRAFFIC CIRCULATION AT THE PROPERTY AS PART OF THIS PROJECT.

**APPLICANT:**  
KIMLEY-HORN & ASSOCIATES, INC.  
1 NORTH LEXINGTON AVENUE, SUITE 505  
WHITE PLAINS, NY 10601  
CONTACT: DEAN APOSTOLERIS  
PHONE: (914) 368-9199  
EMAIL: DEAN.APOSTOLERIS@KIMLEY-HORN.COM

**SITE PARTNER:**  
STOP & SHOP  
1385 HANCOCK ST  
QUINCY, MA 02169  
CONTACT: LINDA CAMARA  
PHONE: (508)-654-6851  
EMAIL: LCAMARA@STOPANDSHOP.COM

**PROGRAM MANAGER:**  
KIMLEY-HORN & ASSOCIATES  
CONTACT: RYAN GRAM  
PHONE: (615)-564-2865  
EMAIL: RYAN.GRAM@KIMLEY-HORN.COM

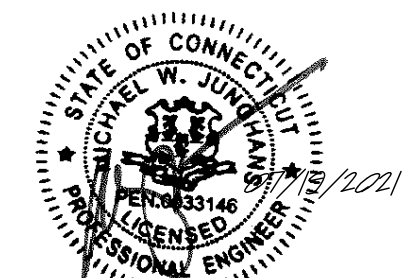
**CIVIL ENGINEER:**  
KIMLEY-HORN & ASSOCIATES  
CONTACT: MIKE JUNGHANS  
PHONE: (914)-368-9189  
EMAIL: MIKE.JUNGHANS@KIMLEY-HORN.COM

**ELECTRICAL ENGINEER:**  
KIMLEY-HORN & ASSOCIATES  
CONTACT: JEFFREY SALLEE  
PHONE: (757)-213-8635  
EMAIL: JEFFREY.SALLEE@KIMLEY-HORN.COM

REV	DATE	DESCRIPTION	BY
1	04/06/2021	CD90S	UK
2	06/04/2021	CD100	VRP
3	07/23/2021	CD100S REVISIONS PER VOLTA COMMENTS	VRP

ISSUE DATE  
**06/04/2021**

ISSUED FOR  
**PERMIT**



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

**STOP AND SHOP #683**  
**180 DANBURY ROAD  
NEW MILFORD, CT 06776**

SHEET TITLE  
**COVER SHEET**

SHEET NUMBER  
**C0-00**

**CONTRACTOR VERIFICATION CHECKLIST**

**CODE BLOCK**

**PROJECT DESCRIPTION**

**PROJECT TEAM**



Sheet Number	Sheet Title
C0-00	COVER SHEET
C0-01	GENERAL NOTES
C0-02	VOLTA STATION OVERVIEW
C1-00	OVERALL SITE PLAN
C2-00	ENLARGED SITE PLAN
C3-00	SITE DETAILS
C3-01	SITE DETAILS
C3-02	SITE DETAILS
C3-03	SITE DETAILS
E1-00	ELECTRICAL ONE LINE DIAGRAM & PANEL SCHEDULE
E2-00	ELECTRICAL NOTES & DETAILS

**811** Know what's BELOW. CALL before you dig.

**DIG ALERT** CALL AT LEAST TWO WORKING DAYS BEFORE YOU DIG

CONTRACTOR SHALL VERIFY ALL PLANS & EXISTING LOCATIONS, CONDITIONS ON THE JOB SITE & SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.

LOCATION MAP

VICINITY MAP

CALL BEFORE YOU DIG

NOTE: THE ORIGINAL SIZE OF THIS PLAN IS 24" X 36". SCALE RATIO IS NOT VALID FOR REDUCED OR ENLARGED SHEET SIZES



**GENERAL NOTES:**

- VOLTA WILL PROVIDE AN INSTALLATION GUIDE AND OTHER SUPPORTING DOCUMENTS AT TIME OF CONSTRUCTION.
- ALL EXISTING CONDITIONS SHOWN ARE APPROXIMATE. EXISTING UTILITY LOCATIONS AND CROSSINGS ARE TO BE LOCATED IN THE FIELD. CONTRACTOR IS TO CONTACT 811 UTILITY PRIOR TO BEGINNING ANY EXCAVATION WORK.
- ALL PAVEMENT, LANDSCAPING, UTILITIES, AND OWNER PROPERTY THAT IS DAMAGED OR AFFECTED BY CONSTRUCTION SHALL BE RETURNED TO EXISTING CONDITIONS OR BETTER AT THE CONTRACTOR'S EXPENSE.
- PROPOSED PAVEMENT STRIPING SHALL LINE UP WITH EXISTING STRIPING WHEREVER POSSIBLE. ADDITIONAL PAVEMENT STRIPE IS NOT NECESSARILY PARALLEL TO THE CONSTRUCTED CHARGING ISLAND.
- THIS ACCESSIBILITY REVIEW WAS UNDERTAKEN TO IDENTIFY DESIGN FEATURES OF THE PROJECT THAT MAY BE CONSIDERED BY GOVERNMENTAL AGENCIES OR DEPARTMENTS, OR NON-GOVERNMENTAL GROUPS TO BE NON-COMPLIANT WITH THE AMERICANS WITH DISABILITIES ACT OF 1990, REVISED 2010 ADA REGULATIONS AND STANDARDS. THE AMERICANS WITH DISABILITIES ACT OF 1990 IS A FEDERAL CIVIL RIGHTS LAW. THERE IS NO FEDERAL REVIEW PROCESS TO ENSURE FULL COMPLIANCE WITH THE GUIDELINES, EXCEPT THROUGH THE FEDERAL COURT SYSTEM. THE DEPICTIONS, NOTES, AND RECOMMENDATIONS, EXPRESSED ON THIS PLAN ARE BASED ON PROFESSIONAL JUDGEMENT GAINED FROM PAST EXPERIENCE WITH ACCESSIBILITY LAWS, CODES, AND STANDARDS AND THE WORKING INVOLVEMENT TO DEVELOP ACCESSIBILITY STANDARDS THAT WILL MEET OR EXCEED THE APPLICABLE FEDERAL GUIDELINES. ACCORDINGLY, NO CLAIMS OR WARRANTIES, EXPRESSED OR IMPLIED, ARE MADE THAT IN PREPARING THIS PLAN AND PROPOSING RECOMMENDATIONS, THAT ALL POSSIBLE BARRIERS TO ALL PEOPLE HAVE BEEN IDENTIFIED.
- CONTRACTOR SHALL ACHIEVE A MINIMUM OF 1% BUT NO MORE THAN A 2% SLOPE IN ANY DIRECTION WITHIN ADJACENT ACCESSIBLE SPACE AND BLENDED ASPHALT OVERLAY TO EXISTING GRADES AS REQUIRED. CONTRACTOR SHALL PROVIDE A SKETCH TO VOLTA OF PROPOSED LIMITS OF ASPHALT OVERLAY TO ACHIEVE THIS REQUIREMENT PRIOR TO BEGINNING PAVEMENT WORK.
- ACCESSIBLE EV STALLS WERE DESIGNED BASED ON EXISTING CONDITIONS AND WITHOUT THE BENEFIT OF SURVEY DATA. ALL ADA AND LOCAL REQUIREMENTS INCLUDING BUT NOT LIMITED TO SLOPE AND SPACING SHALL BE CONFIRMED BY THE CONTRACTOR AND MET AT THE TIME OF CONSTRUCTION.
- CONTRACTOR TO NOTIFY THE ENGINEER OF ANY DISCREPANCIES IN ACCESSIBILITY PRIOR TO CONSTRUCTION.
- UNDER NO CIRCUMSTANCE IS THE CONTRACTOR TO DISRUPT ANY OPERATIONS AT THE SITE HOST LOCATION, INCLUDING BUT NOT LIMITED TO CUSTOMER DISRUPTION, UTILITIES, AND INFRASTRUCTURE.
- CONTRACTOR SHALL BE RESPONSIBLE TO PROTECT WORK AREAS WITH CONES AND/OR BARRICADES AT ALL TIMES.

**EROSION CONTROL & GRADING NOTES:**

- ADDITIONAL EROSION CONTROL DEVICES TO BE USED AS REQUIRED BY LOCAL INSPECTOR.
- DISTURBED AREAS LEFT IDLE FOR FIVE DAYS, AND NOT TO FINAL GRADE, WILL BE ESTABLISHED TO TEMPORARY VEGETATION. MULCH, TEMPORARY VEGETATION OR PERMANENT VEGETATION SHALL BE COMPLETED ON ALL EXPOSED AREAS WITHIN 14 DAYS AFTER DISTURBANCE. ALL AREAS TO FINAL GRADE WILL BE ESTABLISHED TO PERMANENT VEGETATION UPON COMPLETION.
- WHEN HAND PLANTING, MULCH (HAY OR STRAW) SHOULD BE UNIFORMLY SPREAD OVER SEEDED AREA WITHIN 24 HOURS OF SEEDING. IF UNABLE TO ACCOMPLISH, MULCH SHALL BE USED AS A TEMPORARY COVER, CONCENTRATED FLOW AREAS AND ALL SLOPES STEEPER THAN 2.5:1 AND WITH A HEIGHT OF TEN FEET OR GREATER (DOES NOT APPLY TO RETAINING WALLS), AND CUTS AND FILLS WITHIN BUFFERS, SHALL BE STABILIZED WITH THE APPROPRIATE EROSION CONTROL MATTING OR BLANKETS.
- THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO, OR CONCURRENT WITH, LAND-DISTURBING ACTIVITIES.
- EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION CONTROL AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- SEED ALL DISTURBED AREAS UNLESS OTHERWISE NOTED AS PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT RESULT FROM THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY UNDERGROUND UTILITIES TO REMAIN. THE CONTRACTOR IS TO NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES AND/OR CONFLICTS WITH EXISTING OR PROPOSED UTILITIES PRIOR TO PROCEEDING.
- STOCKPILED TOPSOIL OR FILL MATERIAL IS TO BE TREATED SO THE SEDIMENT RUN-OFF WILL NOT CONTAMINATE SURROUNDING AREAS OR ENTER NEARBY STREAMS. STOCK PILE LOCATIONS SHALL BE COORDINATED WITH THE ENGINEER PRIOR TO GRADING ACTIVITIES. EROSION & SEDIMENT CONTROL PRACTICE SHALL BE INSTALLED PRIOR TO STOCKPILE OPERATIONS.
- CONSTRUCT SILT BARRIERS BEFORE BEGINNING GRADING OPERATIONS.
- MULCH AND SEED ALL DISTURBED AREAS AS SOON AS POSSIBLE AFTER FINAL GRADING IS COMPLETED (WITHIN 15 DAYS OF ACHIEVED FINAL GRADES) UNLESS OTHERWISE INDICATED. CONTRACTOR SHALL TAKE WHATEVER MEANS NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION. STEEP SLOPES (GREATER THAN 3:1) SHALL BE STABILIZED WITHIN 7 DAYS OF FINAL GRADING.
- PROVIDE TEMPORARY CONSTRUCTION ACCESS(ES) AT THE POINT(S) WHERE CONSTRUCTION VEHICLES EXIT THE CONSTRUCTION AREA. MAINTAIN PUBLIC ROADWAYS FREE OF TRACKED MUD AND DIRT.
- DO NOT DISTURB VEGETATION OR REMOVE TREES EXCEPT WHEN NECESSARY FOR GRADING PURPOSES.

**ADA COMPLIANCE:**

- CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED BASED ON THE CITY STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
- PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
- ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
- BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESS AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
- CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.

**SITE NOTES:**

- HORIZONTAL DIRECTIONAL DRILLING (HDD) OR OTHER TRENCHLESS METHODS AS APPROVED BY SITE HOST ARE THE PREFERRED METHOD TO INSTALL CONDUIT BENEATH EXISTING PARKING LOTS AND PAVED AREAS.
  - CONDUIT SHALL BE INSTALLED AT A MINIMUM DEPTH OF TWO AND ONE-HALF FEET (2.5') OR BELOW THE FREEZE LINE, WHICHEVER IS DEEPER. CONDUIT TYPE AND DESIGN TO BE SPECIFIED BY EV CHARGING STATION VENDOR AND MEET ALL LOCAL REQUIREMENTS. CONDUIT DIAMETER SHALL BE NO LARGER THAN TWO (2) INCHES.
  - THE RECEIVING PIT SHALL BE LOCATED AS CLOSE AS REASONABLY POSSIBLE TO THE PROPOSED WALL PENETRATION TO LIMIT THE LENGTH OF BUILDING-MOUNTED CONDUIT. LOCATE RECEIVING PIT WITHIN ASPHALT PAVED AREA OR CONCRETE SIDEWALK AREA; RECEIVING PIT SHALL NOT BE LOCATED WITHIN THE UNLOADING PAD (SIX TO TEN INCH (6-10") REINFORCED CONCRETE SLAB AT THE REAR OF THE STORE), RECEIVING PIT LOCATION AND WORK AREA SHALL NOT AFFECT SITE HOST CUSTOMER OR DELIVERY TRAFFIC. SEE SUPPLEMENTAL DOCUMENTS, RECEIVING AREA DIAGRAM.
  - THE RECEIVING PIT SIZE SHALL BE LIMITED TO THREE FEET (3') BY THREE FEET (3') AND SHALL NOT UNDERMINE THE BUILDING FOUNDATION, ENCLOSURES OR CONCRETE UNLOADING PAD.
  - BACKFILL EXCAVATIONS AND REPAIR PAVEMENT PER SPECIFICATIONS BELOW.
  - WHERE CONCRETE PAVEMENT, SIDEWALK, ASPHALT PAVEMENT, CURBING, OR CURBING GUTTER IS REMOVED, THE WIDTH OF THE REMOVAL SHALL EXCEED THE ACTUAL WIDTH AT THE TOP OF THE TRENCH BY TWELVE INCHES (12") ON EACH SIDE OF THE TRENCH, OR A TOTAL OF TWO FEET (2') WIDER THAN THE TRENCH.
  - TRENCHING THROUGH THE CONCRETE RECEIVING PAD AT THE REAR OF THE STORE OR THE DRIVE-THRU SLAB IS NOT ALLOWED. ONLY TRENCHING THROUGH MINOR CONCRETE INSTALLATIONS SUCH AS SIDEWALKS WILL BE PERMITTED.
  - EXCAVATE TRENCHES TO A DEPTH FOUR INCHES (4") DEEPER THAN BOTTOM OF FINISHED PIPE ELEVATION.
  - THE BOTTOM WIDTH OF THE TRENCH SHALL BE AS REQUIRED TO PERMIT CONDUIT TO BE PROPERLY LAIN AND BACKFILL TO BE PLACED AND PROPERLY COMPACTED.
  - REMOVED PAVEMENT, CONCRETE AND EXCAVATED MATERIALS UNSUITABLE FOR USE AS BACKFILL SHALL BE DISPOSED OFFSITE.
  - BEDDING AND BACKFILL MAY BE MATERIAL EXCAVATED FROM THE TRENCH PROVIDED THAT IT IS FREE FROM DEBRIS AND ROCKS LARGER THAN ONE AND ONE-HALF INCHES (1-1/2").
  - OVER THE PIPE, IN LAYERS NOT EXCEEDING FOUR INCHES (4"), PLACE AND COMPACT SUITABLE FILL MATERIAL TO NINETY-FIVE PERCENT (95%) DRY DENSITY AS DETERMINED BY ASTM D698.
  - COMPACTING EQUIPMENT SHALL BE OF SUCH DESIGN, WEIGHT, AND QUALITY AS IS REQUIRED TO OBTAIN THE DENSITIES SPECIFIED HEREIN OR INDICATED ON THE DESIGN DRAWINGS. AREAS INACCESSIBLE TO SELF-PROPELLED COMPACTING EQUIPMENT SHALL BE COMPACTED OR CONSOLIDATED BY HAND-OPERATED MECHANICAL TAMPERS OR VIBRATORS.
  - RESTORE GRASS, LANDSCAPING, IRRIGATION AND ALL FEATURES TO THEIR PRECONSTRUCTION CONDITION.
- ANY UTILITIES, PAVEMENT, IRRIGATION, LANDSCAPING OR OTHER SITE FEATURES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY EV CHARGING STATION VENDOR TO SITE HOST SPECIFICATION.
  - WHERE LANDSCAPING IS IMPACTED, IT IS THE RESPONSIBILITY OF EV CHARGING STATION VENDOR TO REPOSITION OR PROVIDE NEW LANDSCAPING WITHIN THE SITE HOST PROPERTY TO ENSURE COMPLIANCE WITH ANY CODE REQUIREMENTS.
  - WHERE PARKING LOT, SIDEWALK OR OTHER PAVED AREAS ARE IMPACTED OR DAMAGED, IT IS THE RESPONSIBILITY OF THE EV CHARGING STATION VENDOR TO REPAIR THE AREA TO LIKE NEW CONDITION. REPAIR SHOULD EXTEND BEYOND DAMAGED AREA TO NEAREST CLEAN BREAK THAT ALIGNS WITH ARCHITECTURAL BREAKS, MATERIAL JOINTS, PAVEMENT MARKINGS, ETC.
- WHERE APPLICABLE, UTILITY SERVICE PROVIDER TO USE SITE HOST APPROVED ROE (RIGHT OF ENTRY) AGREEMENT. SITE HOST PROGRAM MANAGER WILL PROVIDE TEMPLATE WHEN NECESSARY.
- ASPHALT PAVEMENT REMOVAL AND REPLACEMENT
  - SAW CUT THE PAVEMENT TO NEAT, STRAIGHT LINES TO THE FULL DEPTH OF THE PAVEMENT. PAVEMENT REMOVAL SHALL EXTEND A MINIMUM OF TWELVE INCHES (12") BEYOND THE EDGES OF THE REMOVAL AREA. ANY OTHER PAVEMENT AREAS DAMAGED DURING REMOVAL SHALL ALSO BE REPAIRED OR REPLACED AS NECESSARY.
  - REMOVE THE PAVEMENT WITHOUT DAMAGING THE PAVEMENT THAT IS TO REMAIN IN-PLACE.
  - IF BASE REPLACEMENT IS REQUIRED, COMPACT THE IN-SITU SOILS TO NINETY-FIVE PERCENT (95%) ASTM D698 AND PLUS OR MINUS TWO PERCENT (2%) OF OPTIMUM MOISTURE CONTENT. REMOVE AND REPLACE ANY UNSUITABLE IN-SITU SOILS.
  - PLACE AND COMPACT BASE MATERIAL TO NINETY-FIVE PERCENT (95%) OF ASTM D698.
  - APPLY PRIME COAT TO AGGREGATE BASE IN COMPLIANCE WITH THE DOT SPECS. PRIME COAT SHALL NOT BE APPLIED MORE THAN TWENTY-FOUR (24) HOURS BEFORE ASPHALT PAVEMENT IS PLACED. APPLICATION RATE TO BE PER THE DOT SPEC.
  - CLEAN AND APPLY TACK COAT TO THE ENDS OF CURBS, EDGES OF CONCRETE SURFACES, EDGES OF MANHOLES AND INLETS AND EDGES OF SAW CUT PAVEMENT THAT WILL REMAIN IN-PLACE.
  - PLACE AND COMPACT HOT-MIX ASPHALT. HOT-MIX ASPHALT THICKNESS SHALL BE THE GREATER OF THE IN-PLACE ASPHALT OR THREE AND ONE-HALF INCHES (3.5"). ASPHALT MIX DESIGN SHALL BE BY THE CONTRACTOR.
  - PLANT MIXED ASPHALT BASE/BINDER COURSE: PROVIDE ONE COURSE LAID TO A MINIMUM COMPACTED THICKNESS OF TWO INCHES (2").
  - PLANT MIXED ASPHALT SURFACE COURSE: PROVIDE ONE COURSE LAID TO A MINIMUM COMPACTED THICKNESS OF ONE AND ONE-HALF INCHES (1-1/2").
  - FOR SMALLER JOBS, IT MAY NOT BE FEASIBLE TO INSTALL BINDER AND SURFACE COURSES, IN WHICH CASE SURFACE COURSE, PLACED AND COMPACTED IN TWO LIFTS, WILL BE ACCEPTED.
  - IF PLACING HOT MIX ASPHALT WITH A SHOVEL, BEGIN PLACING HMA AGAINST THE EDGES OF THE PATCH AND WORKING INWARD. HMA SHOULD NOT BE PLACED IN THE CENTER OF THE PATCH AND RAKED TOWARDS THE EDGES.
  - THE FIRST PASS OF THE ROLLER OR COMPACTION EQUIPMENT SHOULD BE ALONG THE EDGES OF THE PATCH TO PROPERLY FORM THE JOINT. THE ROLLER WHEEL OR COMPACTION EQUIPMENT SHOULD OVERHANG THE EXISTING PAVEMENT ONTO THE PATCH BY SIX INCHES (6"). AFTER THE PERIMETER OF THE PATCH HAS BEEN COMPACTED BEGIN TO WORK TOWARDS THE CENTER OF THE PATCH WITH SUCCESSIVE PASSES OFFSET BY SIX INCHES (6").
  - THE CONTRACTOR SHALL UTILIZE THE APPROPRIATE HEAVY COMPACTION EQUIPMENT TO ACHIEVE THE REQUIRED COMPACTION OF THE ASPHALT.
  - SEAL THE AREA AROUND THE EDGES WITH AN ELASTOMERIC LIQUID ASPHALT SEALER TO PROTECT AGAINST WATER INFILTRATION, INCLUDING ANY INADVERTENT OVERCUTS DURING THE SAW CUTTING PROCEDURE.

**PROJECT LEGEND:**  
(SCALE VARIES PER SHEET)

	DETAIL NO. _____ SHEET NO. _____
	PROPERTY LINE
	BREAK LINE
	EXISTING CURB AND GUTTER
	EXISTING CURB
	EXISTING PARKING STRIPE
	EXISTING CONCRETE PAD
	EXISTING TREE
	EXISTING SHRUB
	EXISTING FIRE HYDRANT
	EXISTING CURB INLET
	EXISTING INLET
	EXISTING CATCH BASIN
	EXISTING MANHOLE
	EXISTING POWER POLE
	EXISTING LIGHT POLE
	EXISTING SIGN
	EXISTING ELECTRICAL ROOM
	PROPOSED ELECTRICAL CONDUIT
	PROPOSED ELECTRICAL JUNCTION BOX
	PROPOSED COMMUNICATIONS CONDUIT
	PROPOSED COMMUNICATIONS JUNCTION BOX
	PROPOSED CURB AND GUTTER
	PROPOSED CURB
	PROPOSED PARKING STRIPE
	PROPOSED CONCRETE WHEEL STOP
	PROPOSED CONCRETE PAD
	PROPOSED TREE PROTECTION
	PROPOSED VOLTA V4 L2 CHARGING STATION
	PROPOSED VOLTA V4 DCFV CHARGING STATION
	PROPOSED VOLTA V4 EVCS W/ 4" PIPE BOLLARDS
	PROPOSED VOLTA V3 CHARGING STATION
	PROPOSED V3 EVCS FOUNDATION W/ 4" PIPE BOLLARDS
	PROPOSED PCS FOUNDATION
	PROPOSED PCS FOUNDATION W/ 4" BOLLARDS
	PROPOSED L2 CHARGING PEDESTAL FOUNDATION
	PROPOSED REMOTE HOLSTER RAISED FOUNDATION W/ 6" CURB
	PROPOSED REMOTE HOLSTER (SQUARE/ ROUND) FOUNDATION
	PROPOSED eBOX & eCLICK
	PROPOSED SIGN POST
	PROPOSED SIGN POST W/ BOLLARD
	PROPOSED POST INSTALLED SIGN POST
	PROPOSED POST INSTALLED SIGN POST W/ BOLLARD
	PROPOSED 4" ISOLATED PIPE BOLLARD

**volta**

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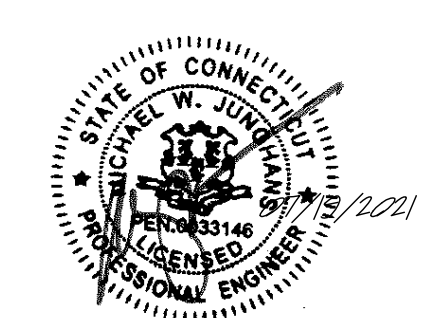
REV	DATE	DESCRIPTION	BY
1	04/06/2021	CD90S	UK
2	06/04/2021	CD100	VRP
3	07/23/2021	CD100S REVISIONS PER VOLTA COMMENTS	VRP

ISSUE DATE

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**STOP AND SHOP #683**

**180 DANBURY ROAD  
NEW MILFORD, CT 06776**

SHEET TITLE

**GENERAL NOTES**

SHEET NUMBER

**C0-01**

NOTE: THE ORIGINAL SIZE OF THIS PLAN IS 24" X 36". SCALE RATIO IS NOT VALID FOR REDUCED OR ENLARGED SHEET SIZES



# Volta Gen4 L2 Station

Volta provides turn-key Electric Vehicle (EV) charging services for premium retail and entertainment destinations. We install and maintain the charging amenity at no cost to site partners as well as EV drivers, driving increased property value and attracting more customers who stay longer.

### VOLTA STATION BENEFITS

- Installation, equipment and maintenance is paid by Volta
- Charges all electric vehicles
- Free electricity supported through third party content on displays
- Volta stations are occupied 80% of the retail day
- Volta has provided 88M free sponsored electric miles, delivered 25 gigawatt hours and eliminated over 39M pounds of CO2 emissions

### CHARGING UNIT INFORMATION *(Single Charging Units)*

- Size: H 85.0" x W 36.5" x D 15.5"
- Display Size: H 48" x W 27"
- Power Type: 208/240VAC, 48A, 10kW max; UL 2202
- Plug: SAE J1772 compliant connector

### POWER REQUIREMENTS

- Charging unit: 60A/2P, 208/240 breaker
- Display/connectivity: 20A/1P, 120V breaker

### INSTALLATION REQUIREMENTS

- Wire Diameter: #6 AWG minimum. Larger for longer conduit runs
- Conduit Diameter: 1.5" minimum per station. Larger conduit required for runs over 250'



55" Media Display

Charges up to 30miles per hour

Universal J1772 connections

Cable Management

Fully Networked



**volta**

155 DE HARO STREET  
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2	06/04/2021	CD100	VRP
3	07/23/2021	CD100S REVISIONS PER VOLTA COMMENTS	VRP

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**STOP AND SHOP  
#683**

**180 DANBURY ROAD  
NEW MILFORD, CT 06776**

SHEET TITLE

**VOLTA STATION  
OVERVIEW**

SHEET NUMBER

**C0-02**

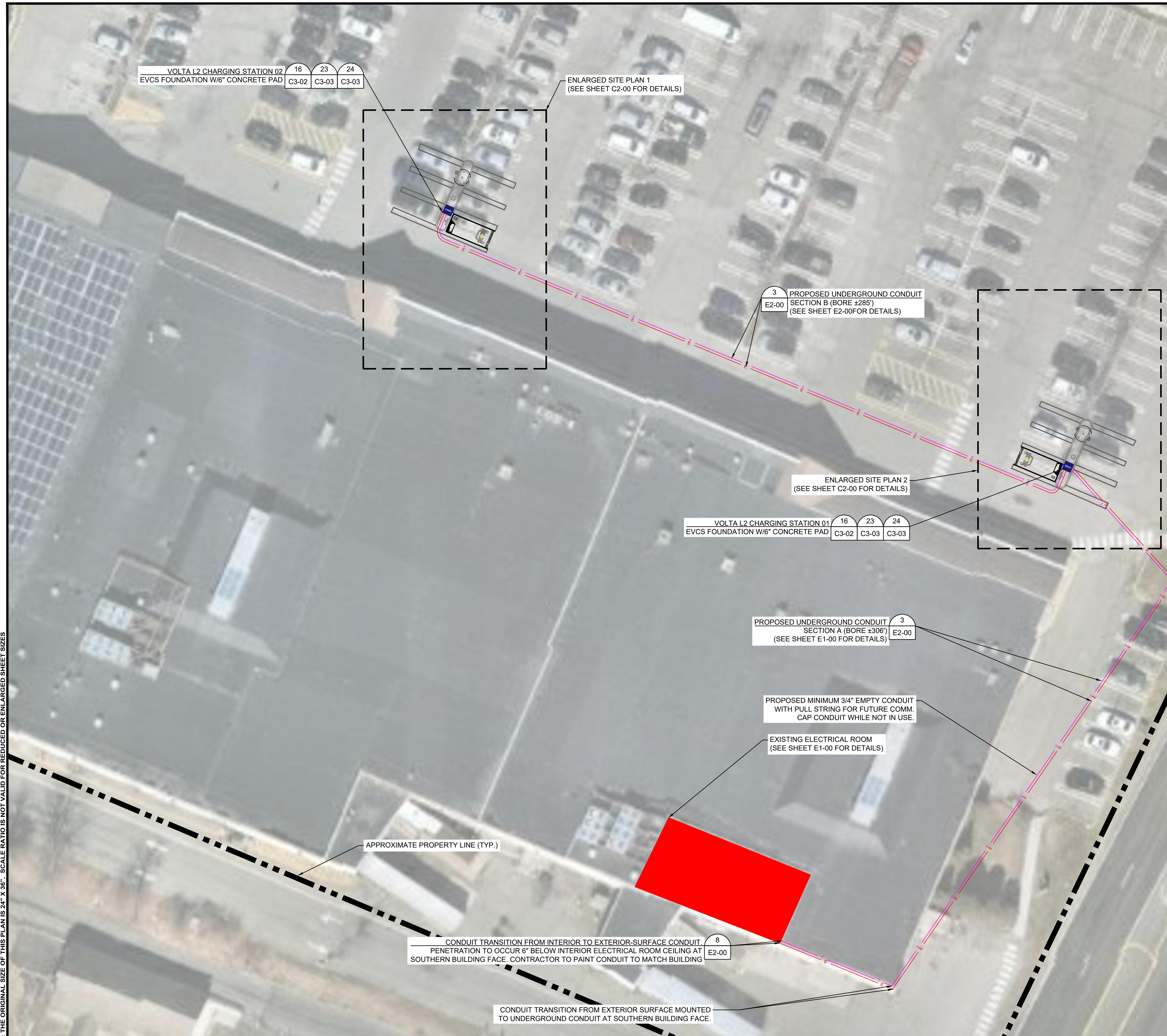
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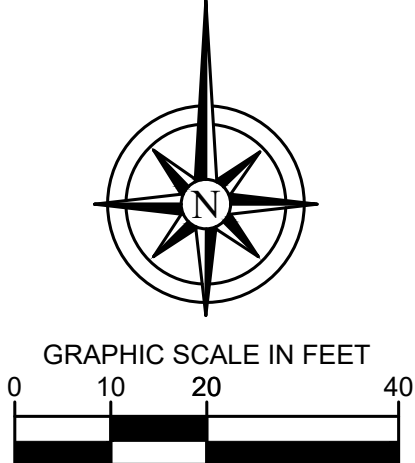
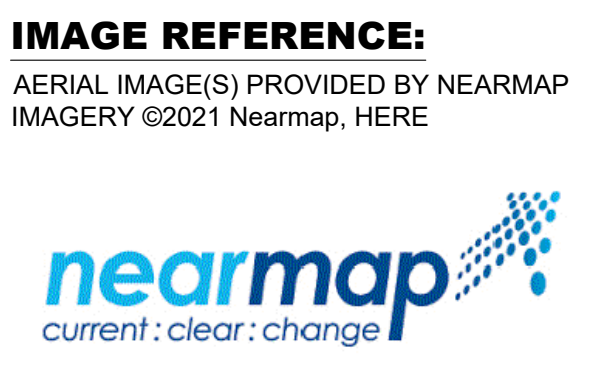
**OVERALL SITE PLAN**

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 CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL FIELD CONDITIONS AND IS TO ALERT THE ENGINEER AND VOLTA OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION. CONTRACTOR TO COORDINATE WITH VOLTA PM FOR ALL FINAL PLACEMENTS OF INFRASTRUCTURE.

- CONSTRUCTION NOTES:**
- CONTRACTOR RESPONSIBILITIES CONSISTS OF, BUT NOT LIMITED TO, CHARGING STATION MOUNTING, FOUNDATION CONSTRUCTION, CONDUIT INSTALLATION, AND WIRING.
  - CONTRACTOR TO INSTALL TREE PROTECTION FENCING PRIOR TO ANY CONSTRUCTION ACTIVITY. SEE SHEET C3-01 FOR DETAILS.
  - CONTRACTOR TO INSTALL, TREE PROTECTION FENCING PRIOR TO ANY CONSTRUCTION ACTIVITY. SEE SHEET C3-01 FOR DETAILS.
  - EXACT STATION PLACEMENT AND ROTATION ANGLE MAY VARY SLIGHTLY UPON INSTALLATION DEPENDING ON SITE CONDITIONS.
  - CONTRACTOR TO FIELD VERIFY ALL STALL DIMENSIONS AND ALL EQUIPMENT LOCATIONS TO ENSURE SUFFICIENT SPACE IS AVAILABLE.
  - CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS WHEN DRILLING INTO EXISTING CIP SLAB AND CIP DROP PANELS TO AVOID DAMAGE TO ANY REINFORCING AND EXISTING STRUCTURAL COMPONENTS.
  - USE APPROVED ASTM METHOD (X-RAY, PACOMETER, GPR, ETC.) TO LOCATE MILD STEEL AND PRE-STRESSING TENDONS PRIOR TO DRILLING. DO NOT CUT OR DRILL THROUGH ANY EXISTING REINFORCING. ADJUST LOCATION AS NECESSARY TO AVOID EXISTING REINFORCING. ENSURE 1" GAP MIN. BETWEEN REBAR AND ANCHORAGE.
  - VOLTA WILL MAKE EVERY EFFORT TO FOLLOW, WITH THEIR PROPOSED CONDUIT, AN EXISTING CONDUIT ROUTE FROM ELECTRICAL ROOM TO PROPOSED STATION PLACEMENTS. WHEN AN EXISTING ROUTE IS NOT AVAILABLE, VOLTA WILL MAKE EVERY EFFORT TO CONCEAL/HIDE, PAINT AND MINIMIZE VISUAL IMPACT OF CONDUITS ANYWHERE THEY MAY BE VISIBLE TO THE PUBLIC.
  - CONTRACTOR IS RESPONSIBLE TO LOCATE ALL VERTICAL AND HORIZONTAL UTILITIES PRIOR TO DIRECTIONAL BORING. ANY ALTERATIONS TO THE PROPOSED CONDUIT ROUTE ARE TO BE COORDINATED WITH THE PROFESSIONAL ENGINEER(S) PRIOR TO CONSTRUCTION.
  - ANY ITEMS TO REMAIN THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED TO THE EXISTING CONDITION OR BETTER AT THE CONTRACTOR'S EXPENSE.
  - CONTRACTOR TO LOCATE JUNCTION BOX OR APPROVED ALTERNATIVE FOR SITE SPECIFIC RUN LENGTHS AND BENDS.

- PARKING NOTE:**
- FOR THE PURPOSE OF THIS PLAN IT IS ASSUMED THERE IS ADEQUATE PARKING IN EXISTING CONDITIONS TO CONVERT 2 PARKING STALLS TO 2 EV PARKING STALLS.

- REFERENCE NOTE:**
- SEE PROJECT LEGEND ON SHEET C0-01 FOR SYMBOLS AND LINE TYPE DESCRIPTIONS.



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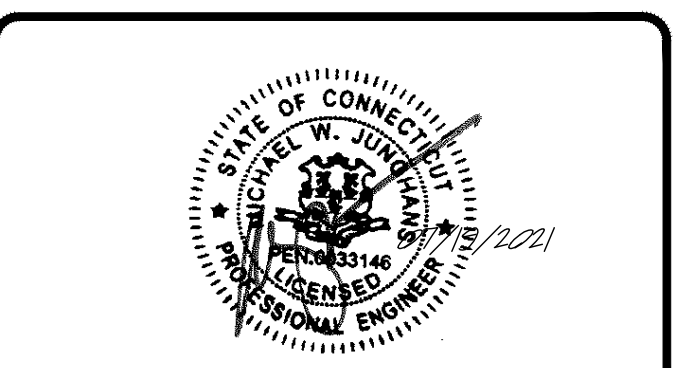
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**STOP AND SHOP  
 #683**

**180 DANBURY ROAD  
 NEW MILFORD, CT 06776**

SHEET TITLE  
**OVERALL SITE PLAN**

SHEET NUMBER  
**C1-00**



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CONTRACTOR TO INSTALL TREE PROTECTION FENCING PRIOR TO ANY CONSTRUCTION ACTIVITY (TYP.) 3 C3-00

EXISTING LIGHT POLE TO REMAIN. CONTRACTOR TO PROTECT IN PLACE

VOLTA L2 CHARGING STATION 02 VARIABLE EVCS FOUNDATION W/6" CONCRETE PAD 16 C3-02 23 C3-03 24 C3-03

VOLTA EV SIGN TO BE MOUNTED TO EXISTING SIGN POST 12 C3-01

EXISTING "CUSTOMER WITH INFANT PARKING" SIGN TO BE SEPARATED. EASTERN FACING SIGN TO BE REMOVED AND RELOCATED/MOUNTED TO THE ADJACENT LIGHTPOLE, FACING EAST, TO SERVE THE ADJACENT PARKING STALL.

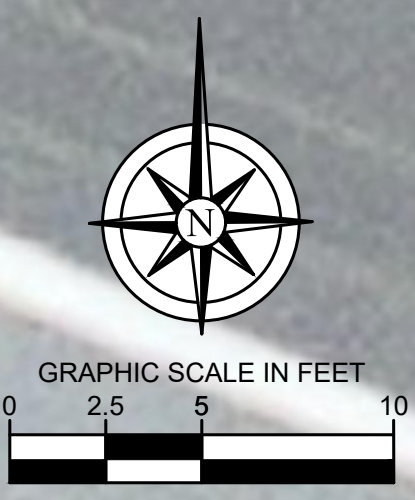
EXISTING CURB TO REMAIN. CONTRACTOR TO PROTECT IN PLACE (TYP.)

EXISTING PARKING STALL TO BE CONVERTED INTO INFANT PARKING STALL

EV STRIPING (TYP.) 5 C3-00

EXISTING PARKING STALL TO BE CONVERTED INTO STANDARD EV STALL

PROPOSED UNDERGROUND CONDUIT SECTION B (BORE ±285) (SEE SHEET E1-00 FOR DETAILS) 3 E2-00



**ENLARGED SITE PLAN**

**1**

CONTRACTOR TO INSTALL TREE PROTECTION FENCING PRIOR TO ANY CONSTRUCTION ACTIVITY (TYP.) 3 C3-00

EXISTING LIGHT POLE TO REMAIN. CONTRACTOR TO PROTECT IN PLACE

EXISTING PARKING STALL TO BE CONVERTED INTO INFANT PARKING STALL

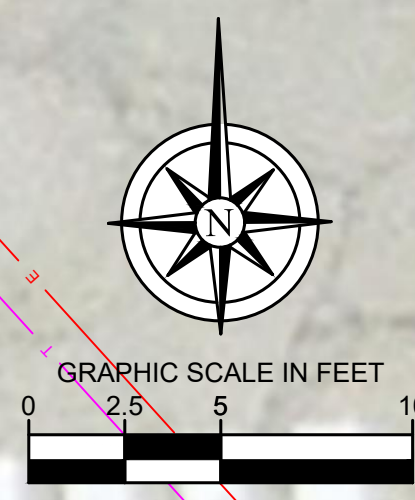
EV STRIPING (TYP.) 5 C3-00

EXISTING PARKING STALL TO BE CONVERTED INTO STANDARD EV STALL

VOLTA EV SIGN TO BE MOUNTED TO EXISTING SIGN POST 12 C3-01

PROPOSED UNDERGROUND CONDUIT SECTION B (BORE ±285) (SEE SHEET E1-00 FOR DETAILS) 3 E2-00

PROPOSED UNDERGROUND CONDUIT SECTION A (BORE ±306) (SEE SHEET E1-00 FOR DETAILS) 3 E2-00



**ENLARGED SITE PLAN**

**2**

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**volta**

155 DE HARO STREET  
 SAN FRANCISCO, CA 94103

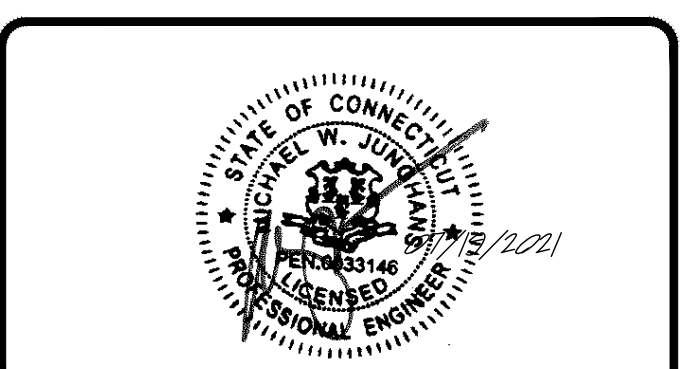
**Kimley»Horn**

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 WHITE PLAINS, NY 10601  
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REV	DATE	DESCRIPTION	BY
1	04/06/2021	CD90S	UK
2	06/04/2021	CD100	VRP
3	07/23/2021	CD100S REVISIONS PER VOLTA COMMENTS	VRP

ISSUE DATE  
**06/04/2021**

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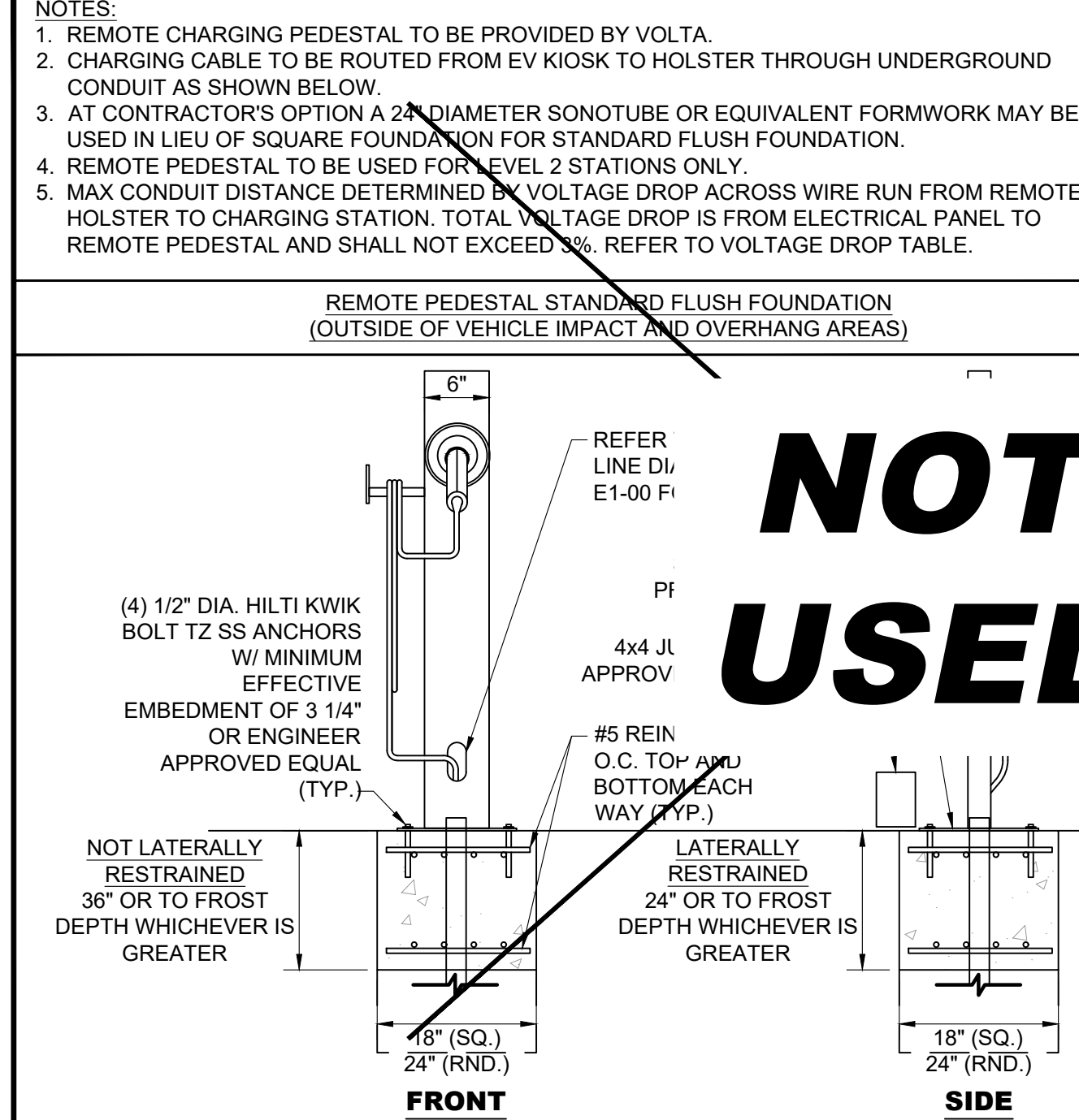
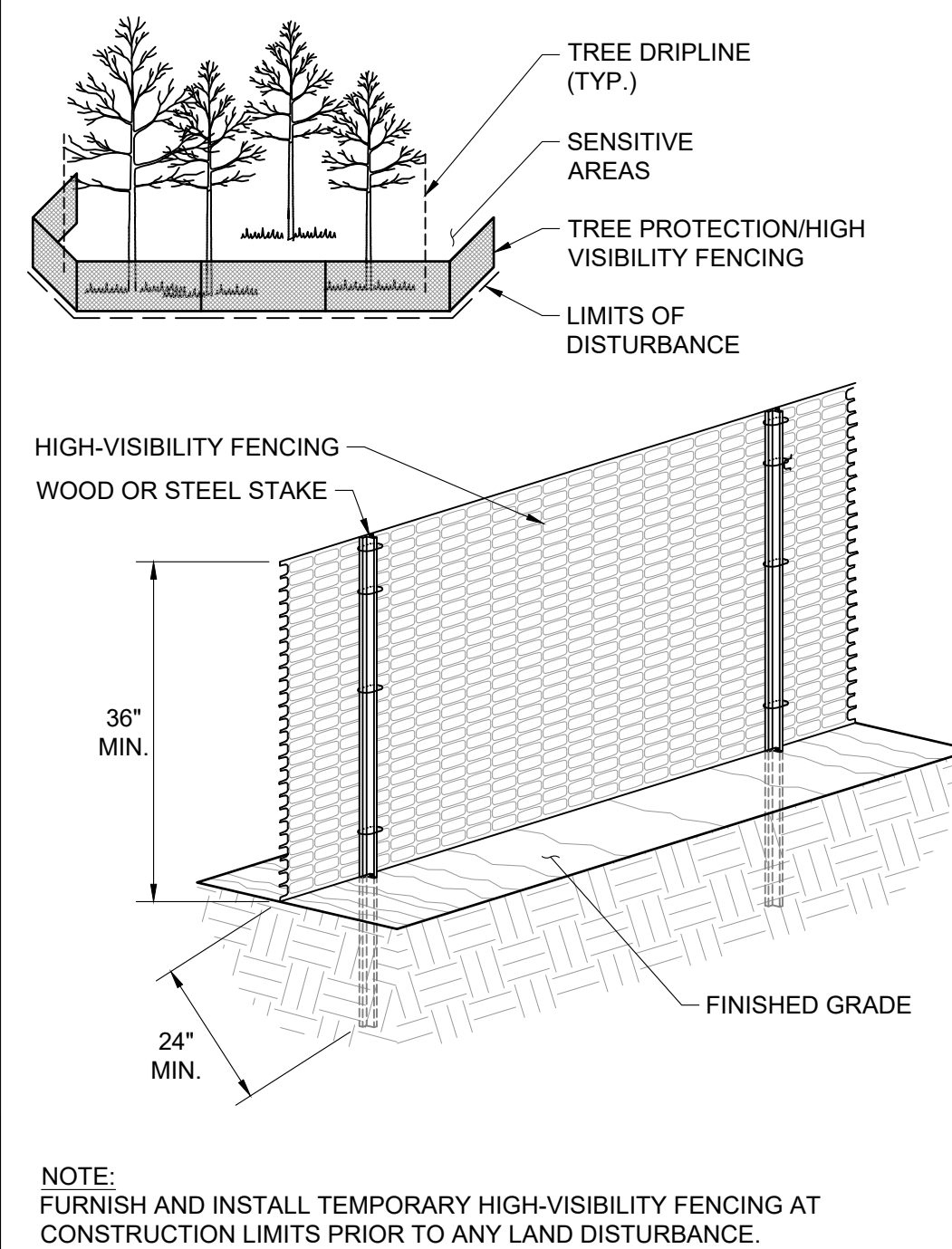
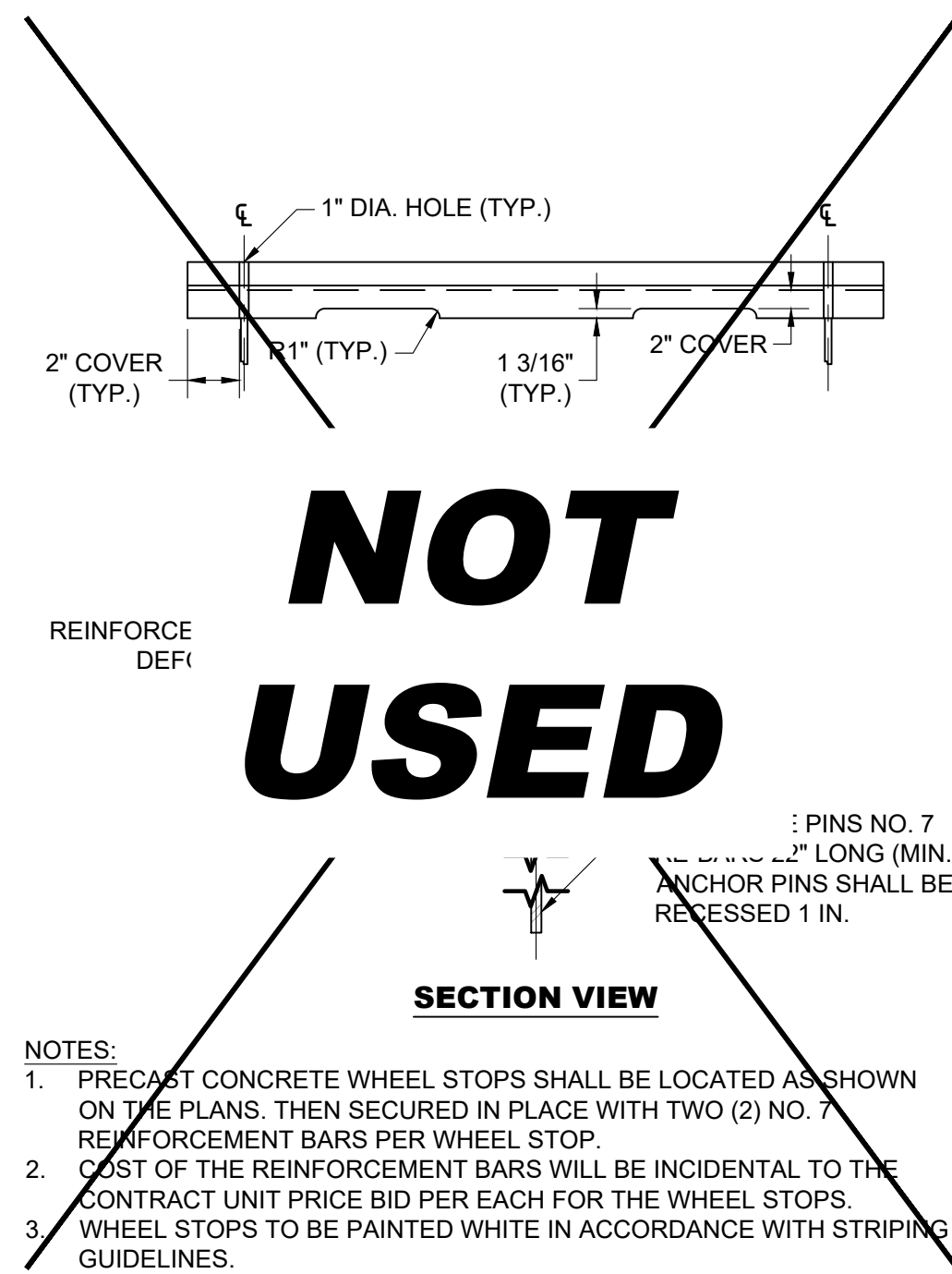
**STOP AND SHOP #683**  
 180 DANBURY ROAD  
 NEW MILFORD, CT 06776

SHEET TITLE  
**ENLARGED SITE PLAN**

SHEET NUMBER  
**C2-00**



- UNLESS NOTED OTHERWISE, THE FOLLOWING NOTES RELATING TO THE "SITE DETAILS" SHEETS SHALL GOVERN.
- COMPRESSIVE STRENGTH OF CONCRETE FOUNDATION SHALL BE A MINIMUM OF 4,500 PSI AT 28 DAYS WITH MAXIMUM W/CM RATIO OF 0.45 AND AIR-CONTENT OF 5% +/- 1.5%.
- MINIMUM YIELD STRENGTH OF REINFORCEMENT TO BE 60,000 PSI (ASTM-A615).
- REFERENCE CIVIL AND ELEC DRAWING FOR EQUIPMENT LAYOUT, LOCATION OF CONDUIT, ETC.
- FINAL ANCHOR BOLT AND POLE DESIGN INCLUDING SIZE AND CONFIGURATION ARE BY MFR.
- BEFORE STARTING ANY WORK, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS ON THE SITE AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- NO GEOTECHNICAL ENGINEERING REPORT WAS PROVIDED BY THE OWNER. FOUNDATION DESIGN IS BASED ON A MINIMUM OF 1,500 PSF NET ALLOWABLE BEARING PRESSURE ON UNDISTURBED NATURAL SOIL OR COMPACTED FILL UNLESS OTHERWISE NOTED, UNLESS OTHERWISE DIRECTED BY THE OWNER, ALL FOUNDATION WORK RELATED TO INSTALLATION OF REBAR SHALL BE INSPECTED BY OTHERS.
- KIMLEY-HORN AND ASSOCIATES, INC. IS NOT RESPONSIBLE FOR THE DESIGN OF THE EQUIPMENT OR ANCHORAGE TO THE FOUNDATION. MANUFACTURER SHALL SUBMIT LOADS TO ENGINEER FOR RECORD KEEPING PURPOSES ONLY.
- DESIGN IS BASED ON THE SPECIFIC EQUIPMENT SHOWN IN THESE DRAWINGS AND ILLUSTRATED ON THE VOLTA CUT SHEETS.
- ALL MATERIALS SHALL BE IN ACCORDANCE WITH THE LOCAL DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
- ALL FOUNDATIONS ARE TO INCLUDE COMPACTED SUBGRADE AND MINIMUM 6" COMPACTED STONE BASE UNLESS OTHERWISE SPECIFIED.
- BUILDING CODE: IBC 2018
  - DESIGN LOADS
  - DEAD LOAD: SELF WEIGHT
  - LIVE LOAD: BOLLARDS ARE NOT DESIGNED FOR FULL 6 KIP IMPACT LOAD UNLESS OTHERWISE NOTED "VEHICULAR RATED"
  - WIND LOAD: ASCE 7-10
    - OCCUPANCY CATEGORY = II
    - WIND SPEED = 170 MPH
    - EXPOSURE = B



START POINT	END POINT	AMPERAGE (A)	VOLTAGE	DISTANCE (FT)	CONDUCTOR (AWG)	CONDUCTOR RESISTANCE	V.D.	V.D. %
EV#	REMOTE PEDESTAL	40	208	5	6	0.51	0.20	0.10
EV#	REMOTE PEDESTAL	40	208	20	6	0.51	0.82	0.39
EV#	REMOTE PEDESTAL	40	208	35	6	0.51	1.43	0.69
EV#	REMOTE PEDESTAL	40	208	50	6	0.51	2.04	0.98

**GENERAL NOTES** SCALE N.T.S. 1

**CONCRETE WHEEL STOP** SCALE N.T.S. 2

**TREE PROTECTION** SCALE N.T.S. 3

**FLUSH REMOTE PEDESTAL FOUNDATION** SCALE N.T.S. 4

SCALE N.T.S. 4

**volta**  
155 DE HARO STREET  
SAN FRANCISCO, CA 94103

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**FOR REFERENCE ONLY, DESIGNED AND PROVIDED BY OTHERS**

Volta Charging  
**STRIPING GUIDELINES**

**PRODUCTS**  
Cement Background: Benjamin Moore Floor & Patio Battleship Blue N122-2X 31x2x30.0 811x0.0 610x30.0  
Asphalt Background: Latex-tie 4.75 Gal. Ultra Shield Driveway Filler Sealer  
Traffic Paint: Sherwin Williams TM2153 LF Yellow TTP-1952D, TM2152 White TTP-1952D

**SURFACE PREP**  
Backgrounds are to only be painted for marquee locations or any location where the existing space has conflicting designations or is poor shape. For all other instances please proceed to branded striping.

**CEMENT BACKGROUND:**  
For cement backgrounds please use battleship blue. All backgrounds must run edge-to-edge across the entire parking space.

**ASPHALT BACKGROUND:**  
Asphalt should be resealed with sealcoat. All backgrounds must run edge-to-edge across the entire parking space.

**BRANDED STRIPING**

**VOLTA LOGO:**  
Should match the overall background color of the parking stall (unless you are omitting the container shape according to other specs, if so paint it white).

**LINES & STENCILS:**  
Use traffic grade yellow for the lightning bolt stencil. Use traffic grade white for all other lines and stencils.

- SHAPE (WHITE)** Place flush with the top left corner.
- VOLTA LOGO** Center within the shape.
- NUMBERS (WHITE)** The right number lines up flush right to the "G" in "CHARGING" and flush top with the Volta logo. There should be 3 inches in-between the left and right numbers. If stall is less than 8 feet, align numbers with the middle of the "G" (See page 2).
- LETTERS** Place centered, 4 inches from the bottom of the stall.
- CAR** Place centered 14 inches from the top of the letters.

1 of 3  
volta Founded in 2010. Designed in San Francisco, built to last in the USA. info@voltacharging.com

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Volta Charging  
**STRIPING GUIDELINES**

**ABSOLUTELY DO NOT**

- Paint only a portion of the background (edge-to-edge or not at all)
- Paint the lightning bolt, any color but yellow or white
- Only put 1 number in top right corner
- Paint the Volta logo any color other than white or battleship blue/Sealcoat

**ACCESSORIES**

- HEEL BLOCKS** Place 8 inches above the logo, centered within the stall. Wheel s should be painted white.
- CESS AISLE** Should be painted white.
- S** No need to paint the curbs unless they tinted an existing or conflicting color. If the case, paint the curb white.

**NOT USED**

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Volta Charging  
**STRIPING GUIDELINES**

**DIAGONAL STALLS**

- SHAPE (SEE PAGE 1)** Omit in diagonal spaces.
- VOLTA LOGO (WHITE)** 6 inches from the top line at the same angle as the stall lines.
- NUMBERS (WHITE)** The right number lines up flush right to the "G" in "CHARGING" and flush top with the Volta logo at same angle as the stall lines. There should be 3 inches in-between the left and right numbers. If stall is less than 8 feet, align numbers with the middle of the "G" (See diagram)
- LETTERS** Place centered, 4 inches from the bottom of the stall according to the top line.

Place centered 14 inches from the top of rs at the same angle as the stall lines.

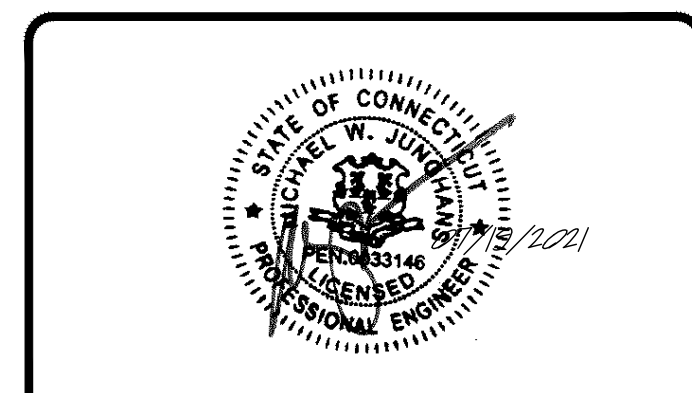
**HEEL BLOCKS** Place 8 inches above the go in the same angle as the stall lines. According to the space left in angled heel Blocks should be painted white.

**NOT USED**

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**STOP AND SHOP #683**  
180 DANBURY ROAD  
NEW MILFORD, CT 06776

SHEET TITLE  
**SITE DETAILS**

SHEET NUMBER  
**C3-00**

**EV STRIPING GUIDELINES** SCALE N.T.S. 5

**EV STRIPING GUIDELINES** SCALE N.T.S. 6

**EV STRIPING GUIDELINES** SCALE N.T.S. 7

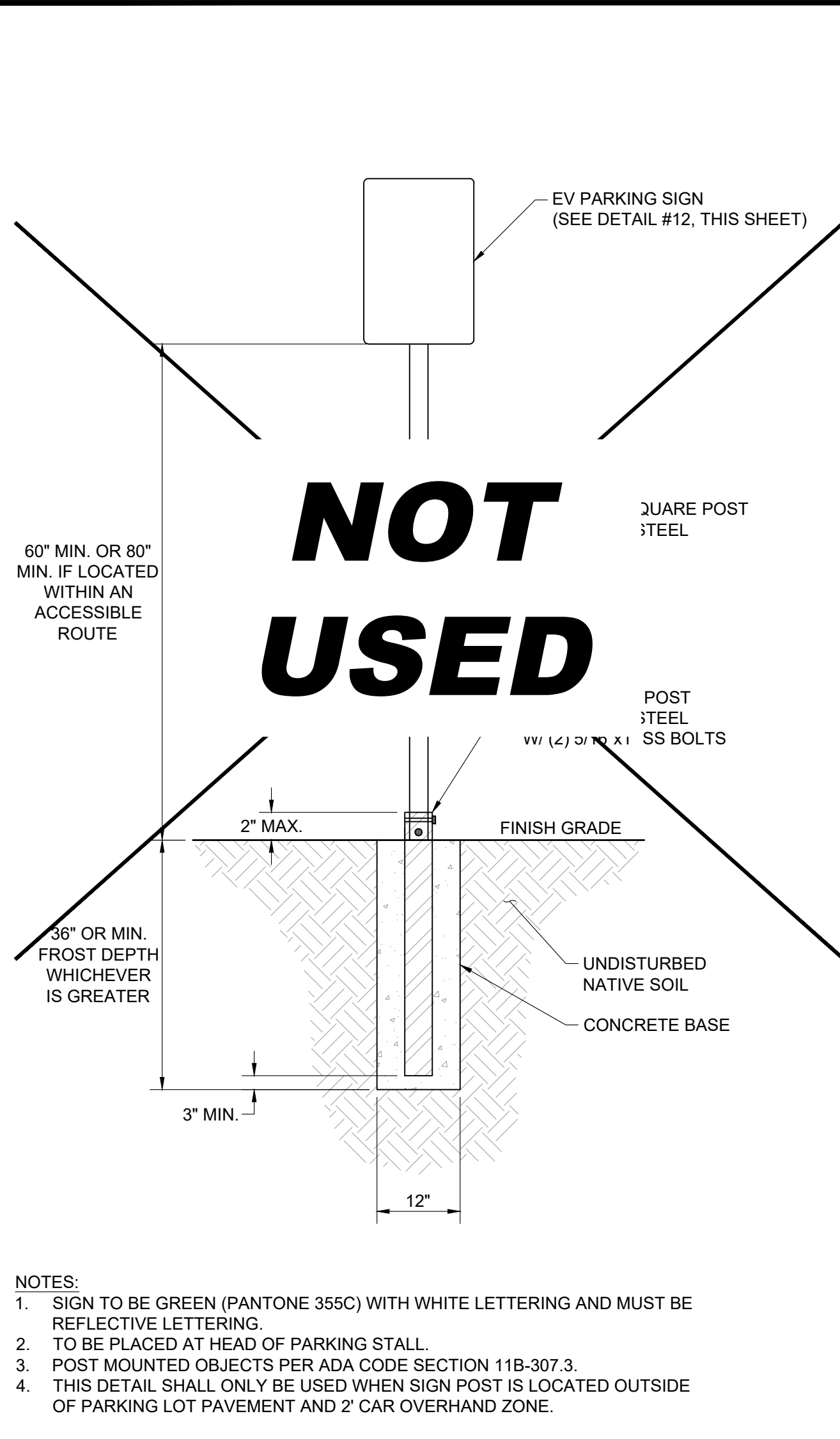
SCALE N.T.S. 7

SCALE N.T.S. 7

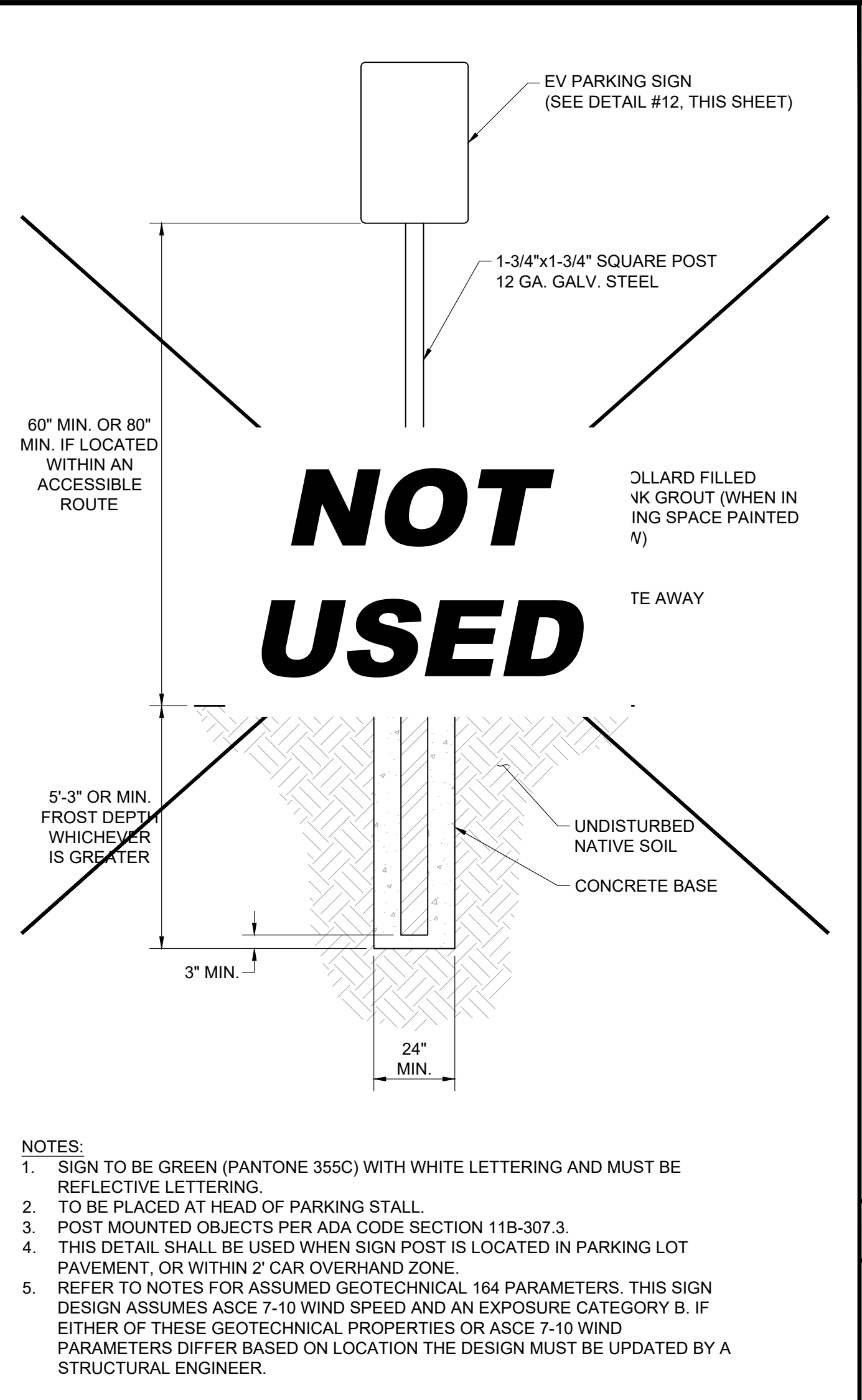
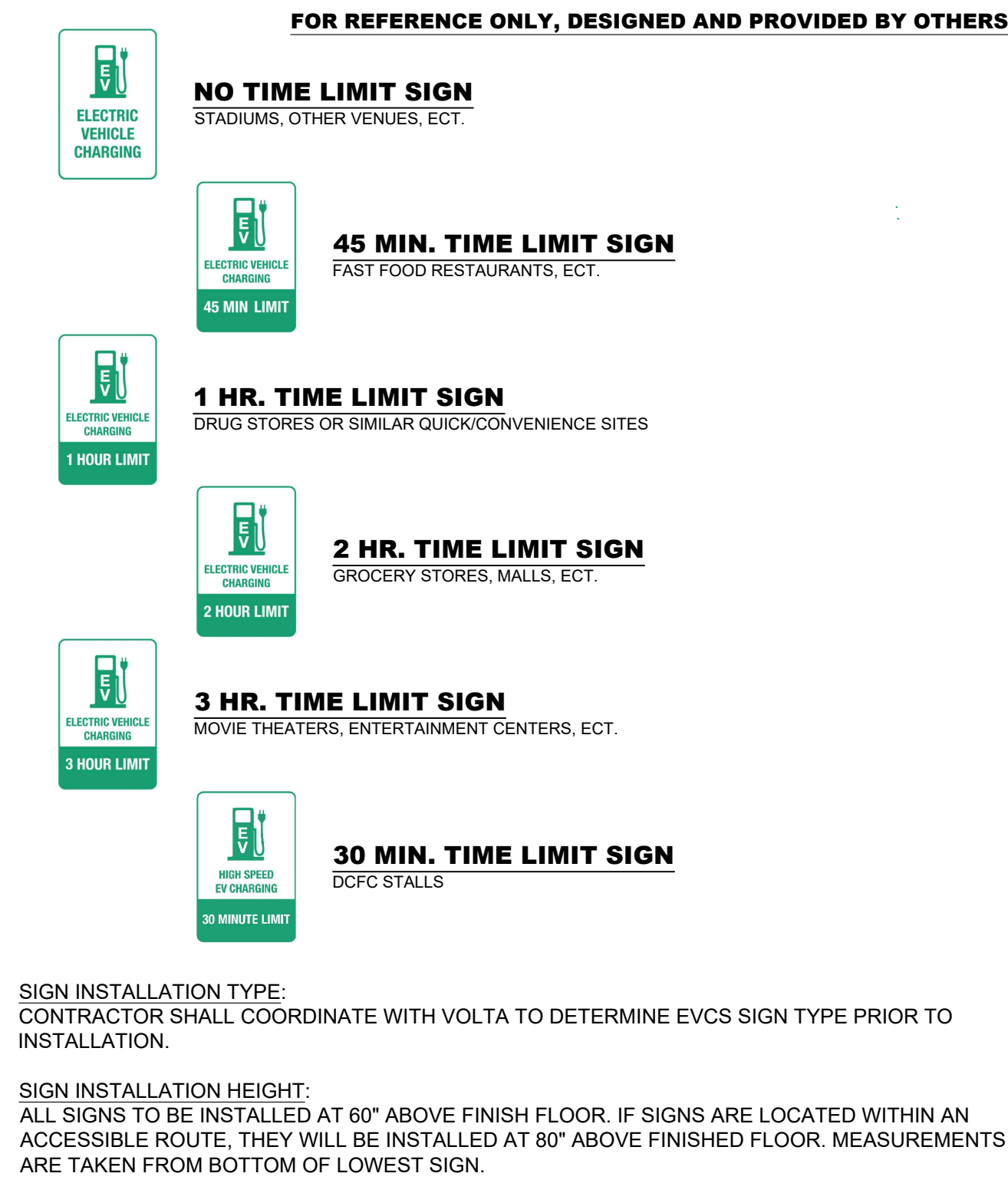
NOTE: THE ORIGINAL SIZE OF THIS PLAN IS 24" X 36". SCALE RATIO IS NOT VALID FOR REDUCED OR ENLARGED SHEET SIZES



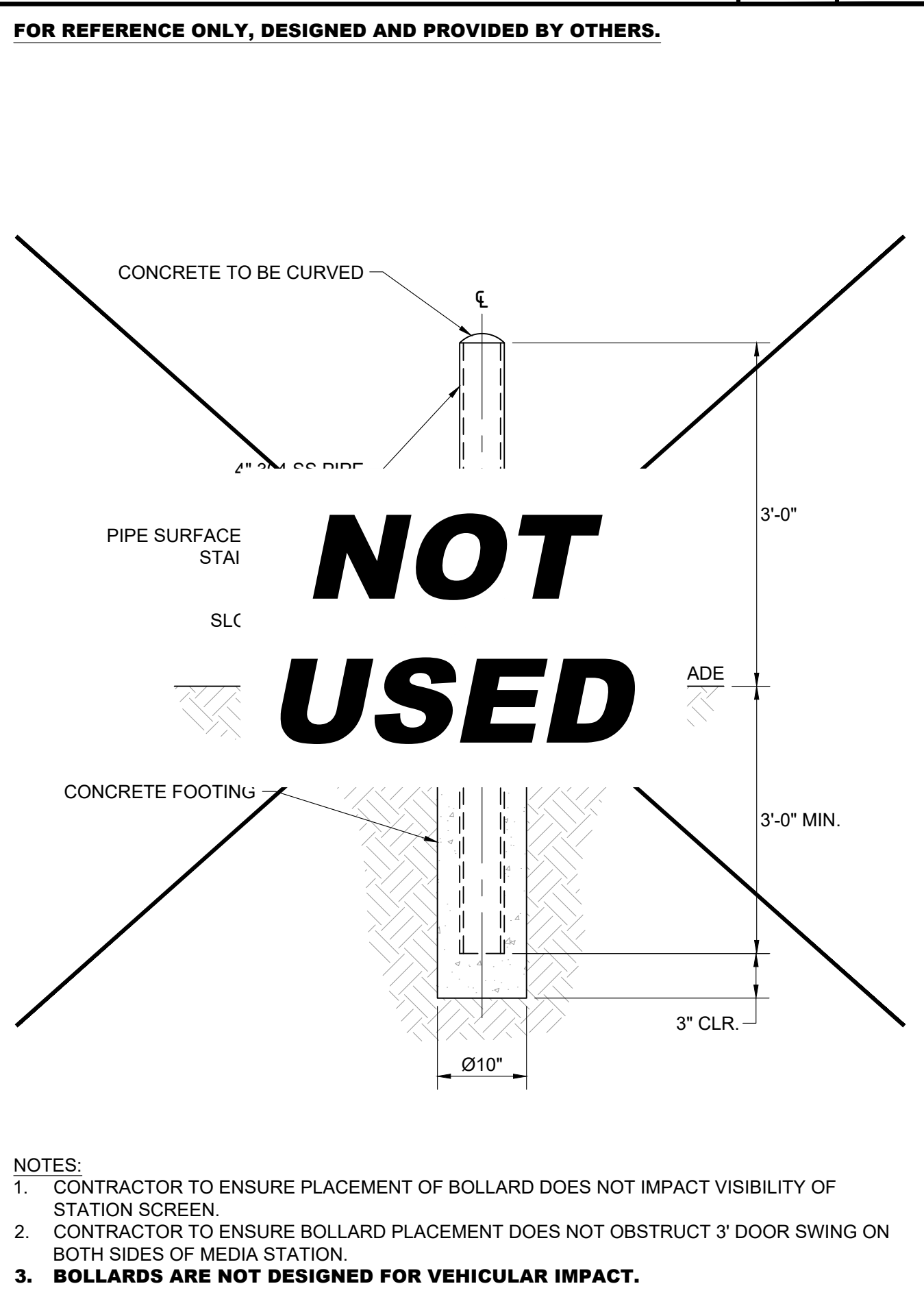
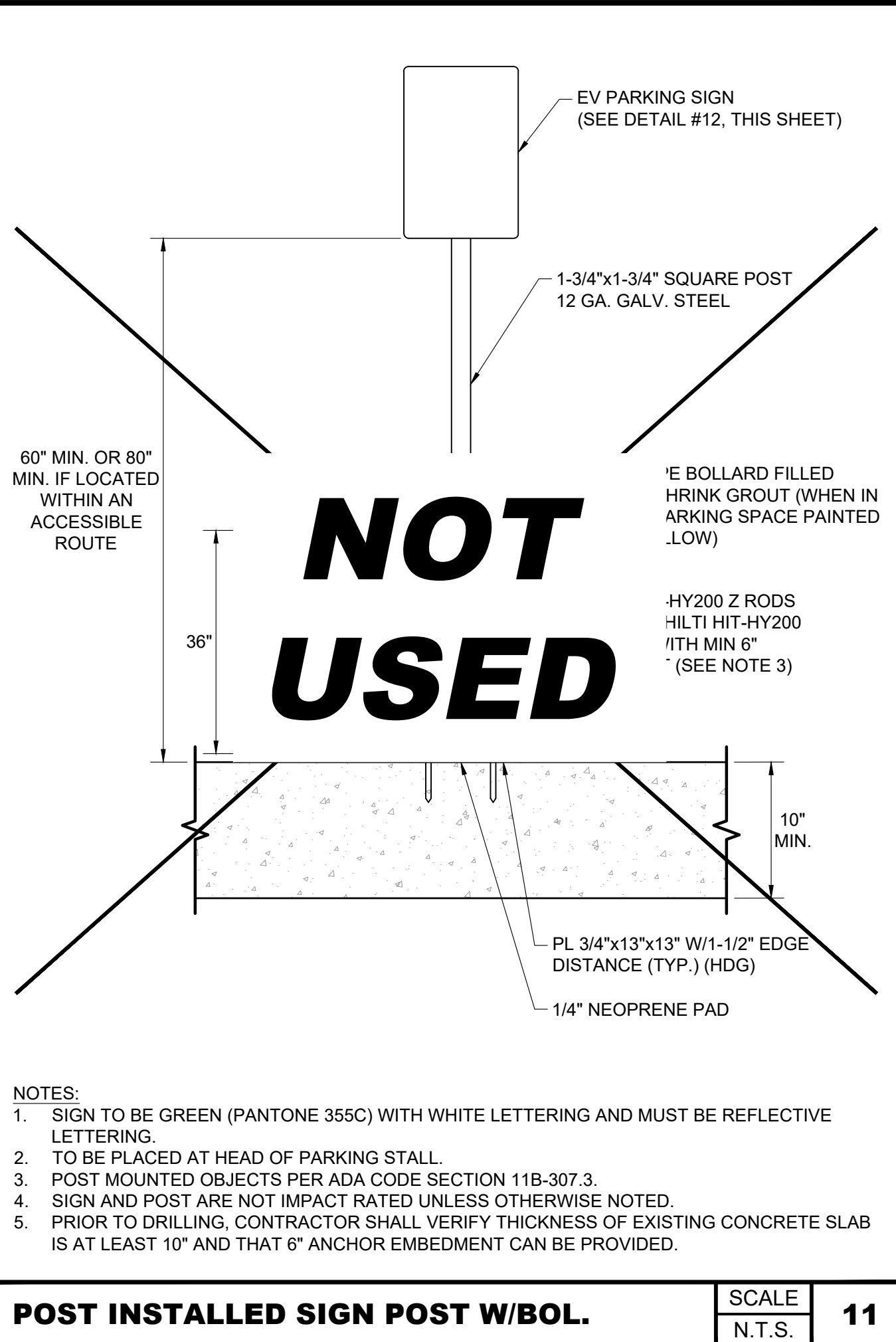
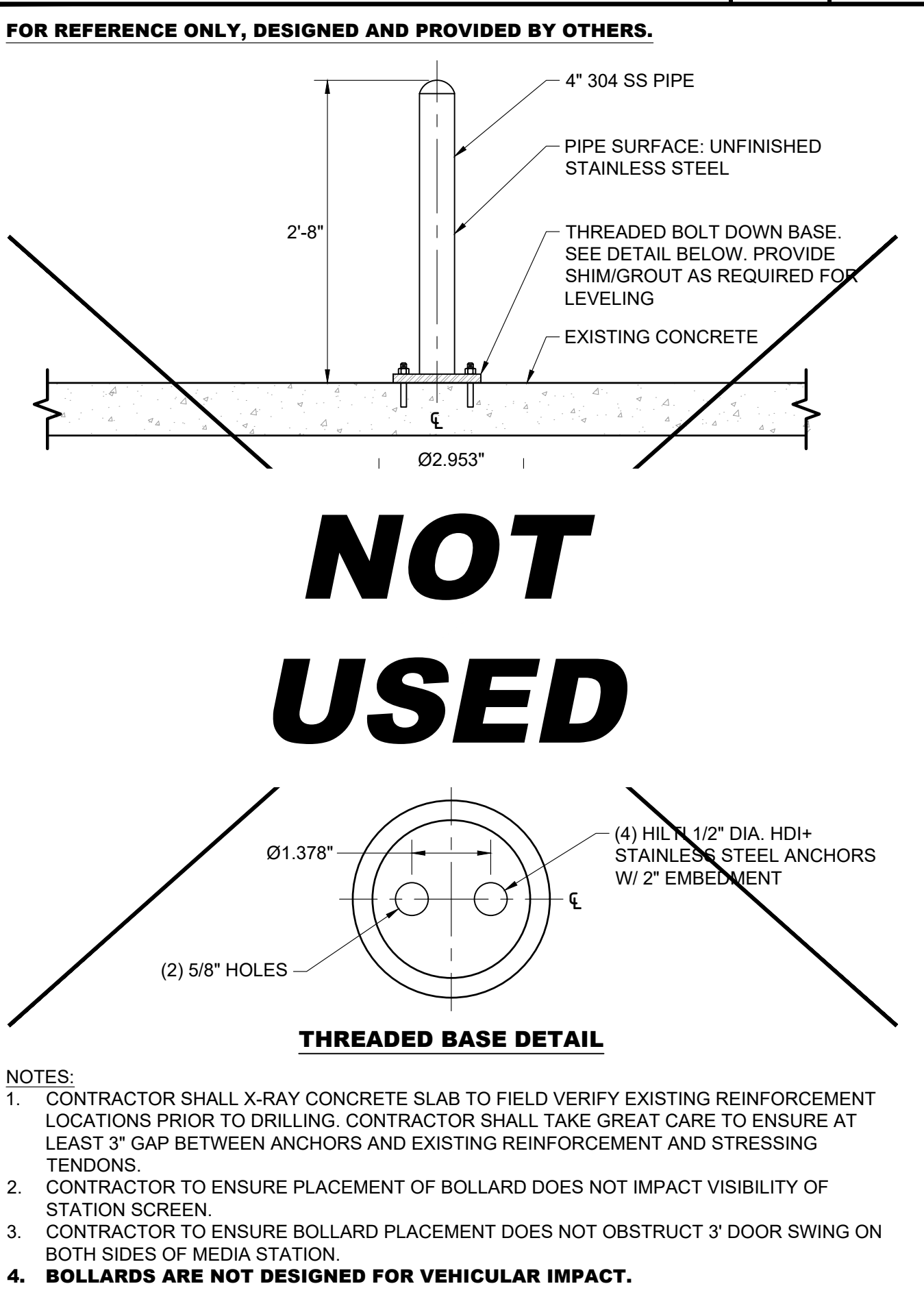
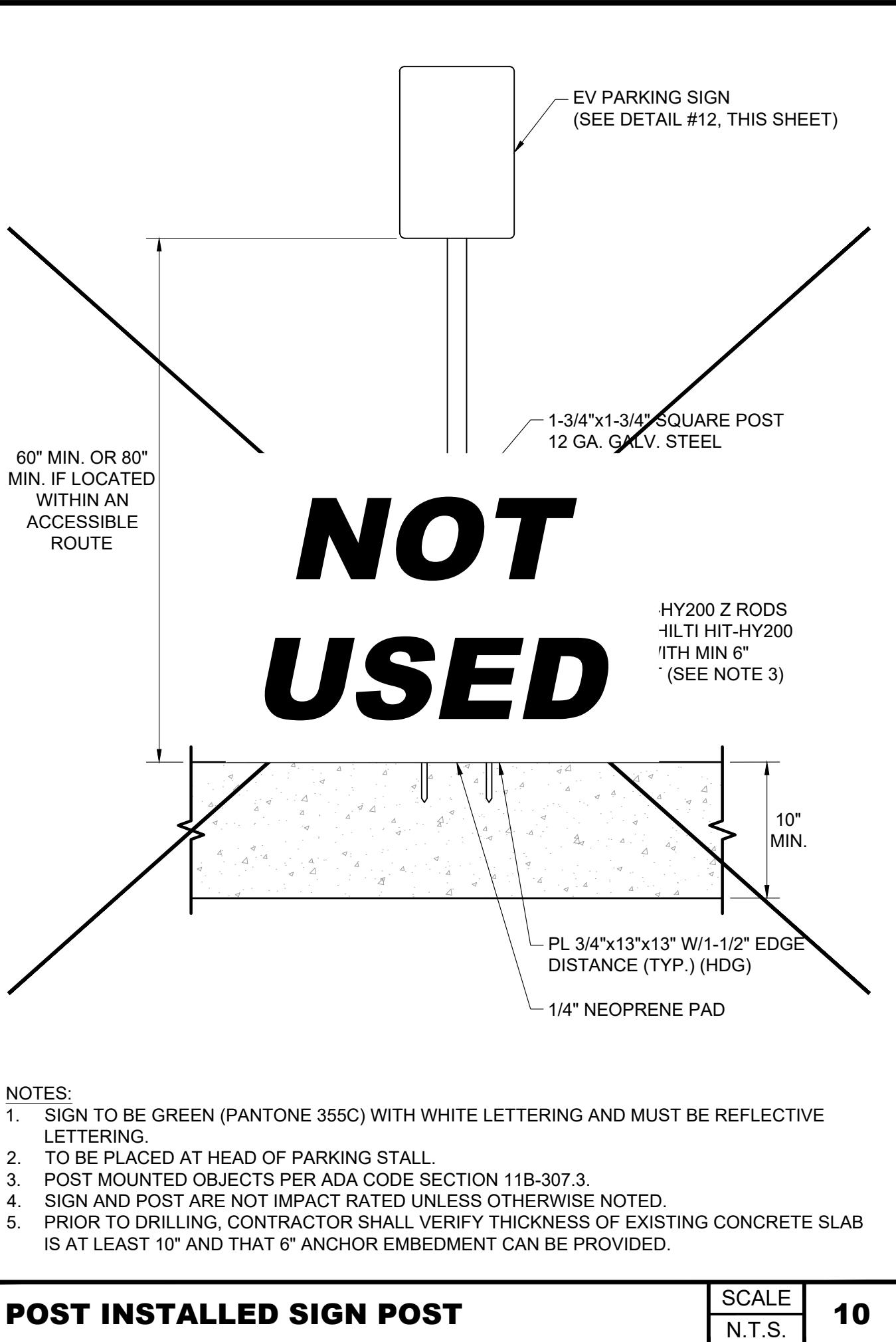
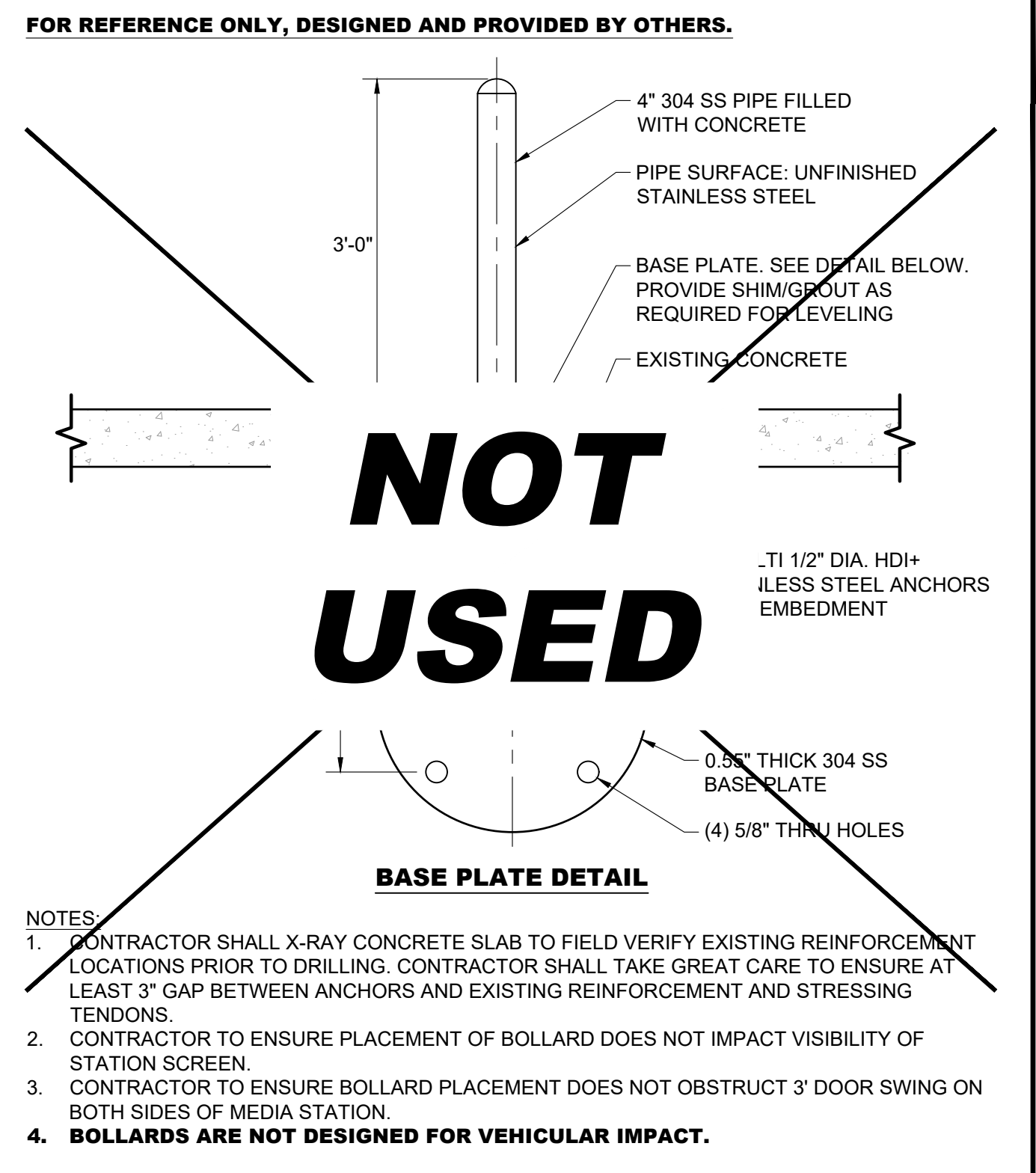
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**SIGN POST** SCALE N.T.S. 8



**SIGN POST W/BOLLARD** SCALE N.T.S. 9



**volta**

155 DE HARO STREET  
SAN FRANCISCO, CA 94103

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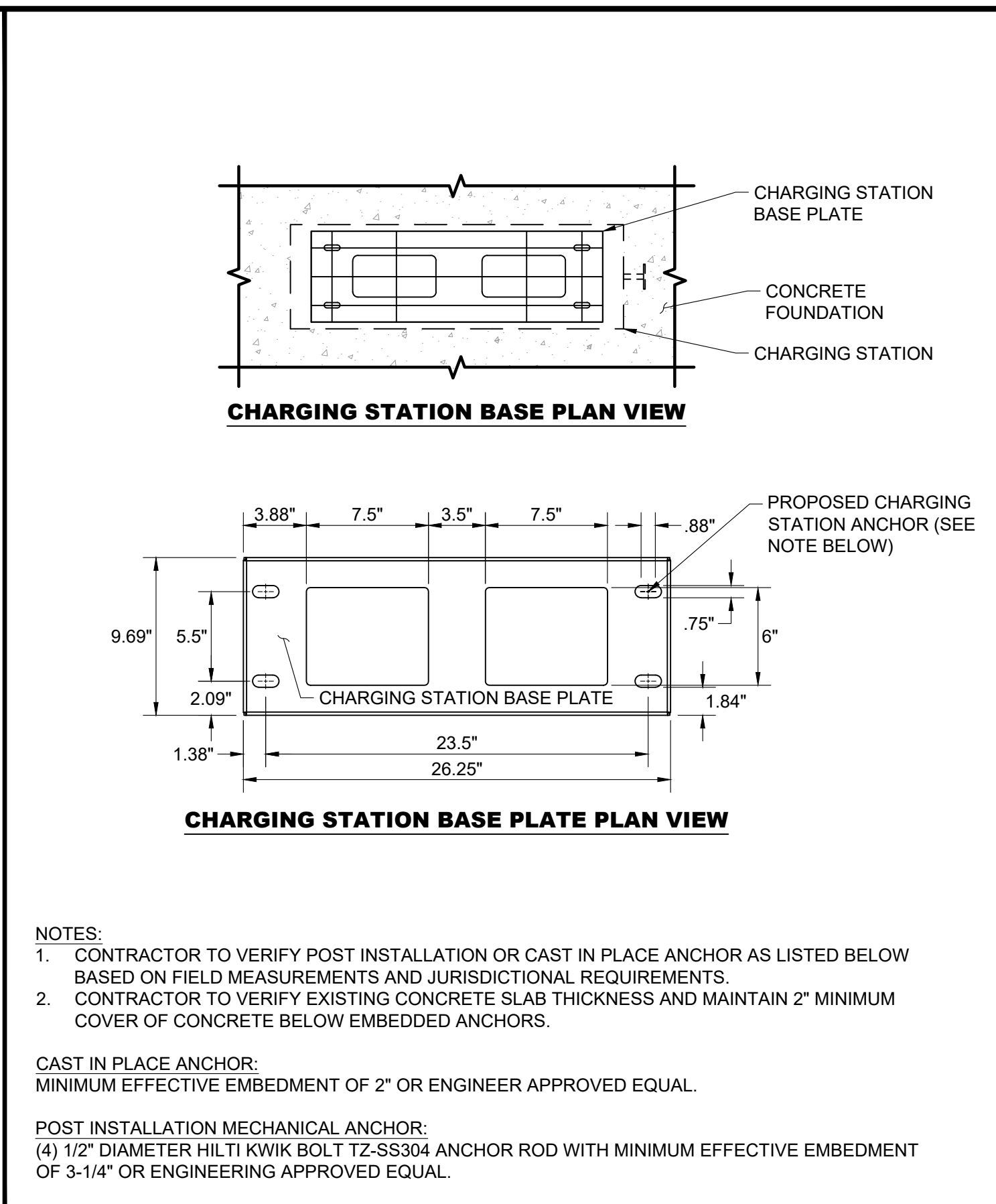
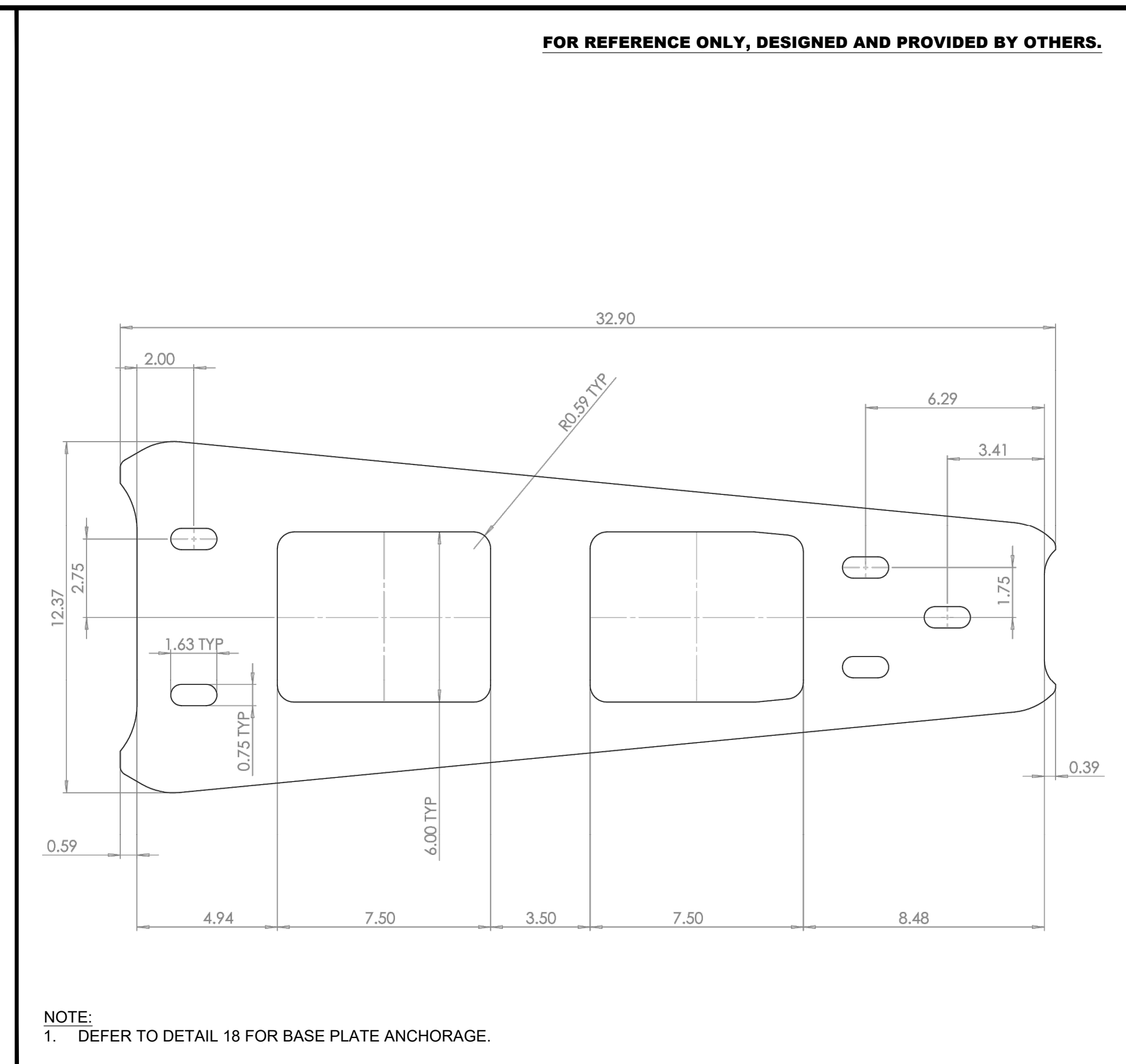
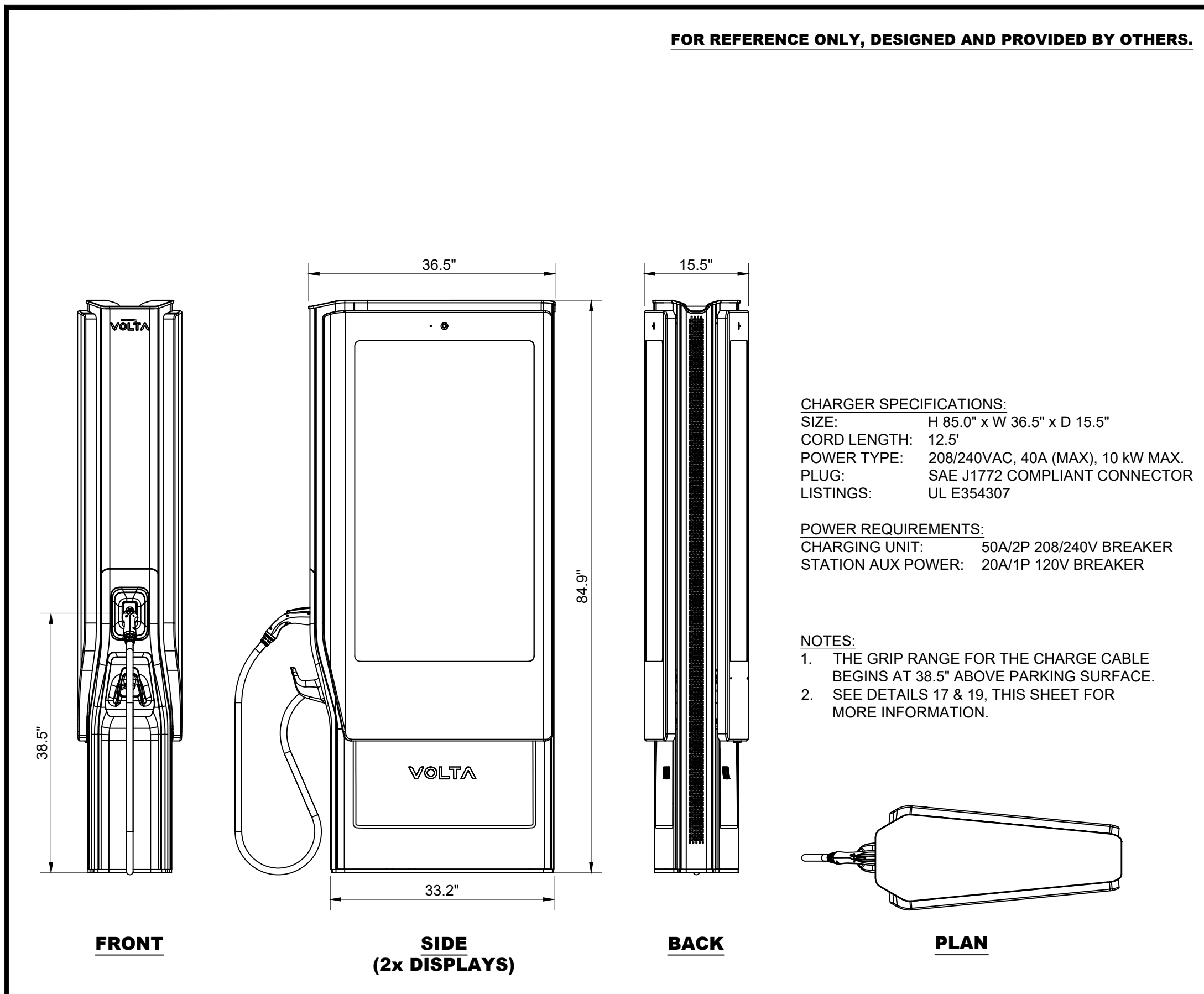
**STOP AND SHOP #683**

180 DANBURY ROAD  
NEW MILFORD, CT 06776

SHEET TITLE  
**SITE DETAILS**

SHEET NUMBER  
**C3-01**





**VOLTA V4 L2 EVCS**      SCALE N.T.S.      **16**

**VOLTA V4 BASE PLATE**      SCALE N.T.S.      **17**

**VOLTA BASE PLATE ANCHORAGE**      SCALE N.T.S.      **18**

**volta**

155 DE HARO STREET  
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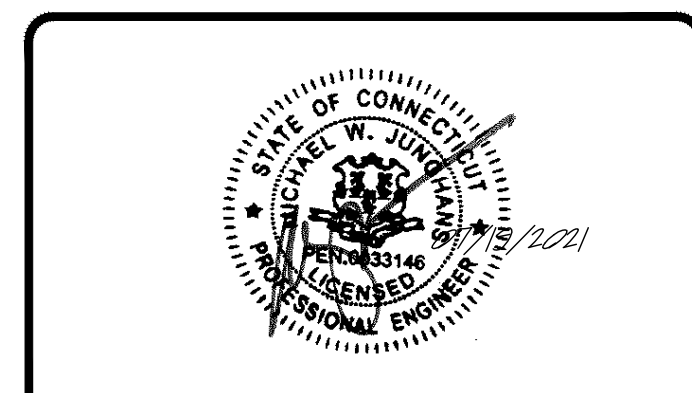
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**180 DANBURY ROAD  
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SHEET TITLE  
**SITE DETAILS**

SHEET NUMBER  
**C3-02**

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**Level 2 Media Station**

Volta Charging is driving the transition to clean electric transportation by transforming properties with electric vehicle charging. No longer will people drive to fuel, but fuel where they drive.

Volta's turn-key electric vehicle charging is tailored to each location's needs and desired customer experience to increase traffic and customer engagement. Our fully integrated EV chargers include high-impact digital media screens that provide properties with branding and messaging as well as additional revenue opportunities.

**Charger Specs**

- Output power: 10 kW max (AC)
- Safety certification: UL 2202

**Power Requirements**

- Input voltage: 208 - 240 VAC
- Output voltage: 208 - 240 VAC
- Circuit size: 60A/2P, 208/240 breaker
- Network connectivity: Cell connection or LAN access

**Display Screen Specs**

- Size: 55" outdoor LED back light system x2
- Picture: Full HD 1080p resolution
- Power requirements: 20A/1P, 120V breaker
- File type: JPEG or PNG

**Installation Requirements**

- Foundation req: 36"L x 36"W x 36"D
- Conduit diameter: 1.5" min per station approx\*
- \*Separate conduit for communication may be required

**Example Level 2 Media Foundation Plans**

770-00002-A

voltacharging.com

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**eMOBILITY SOLUTIONS**

**eClick Data sheet**

**innogy**

**General and electrical specifications**

Specification	eClick
Charging power	up to 10.4kW / 12kW (up to 50A)
Mains input power	208 Volt AC WYE system, 50A, 10.4kW 240 Volt AC split phase, 50A, 12kW 240 Volt AC Center Tap Delta, 50A, 12kW
Output power to eBox	208 Volt AC WYE system, 50A, 10.4kW 240 Volt AC split phase, 50A, 12kW 240 Volt AC Center Tap Delta, 50A, 12kW maximum output depends on local distribution grid
Storage temperature	-22°F to +176°F (-30°C to +80°C)
IP	I
Max line cross section	Max supply line (top or bottom entry): AWG (10 mm²)
Certifications	UL and FCC (tested and confirmed by a certified body)
Packaging dimensions (W x D x H)	1'-8 1/4" x 8-4/5" x 3'-0/5" (255 mm x 225 mm x 75 mm)

**Recommended additional accessories**

Accessory	eClick
Short circuit / overload protection	Circuit breaker required according to national law and regulations
Installed in sub-distribution per charge port	Electrical installer to choose correct circuit breaker depending on grid type and power.

Broadband TelCom Power, Inc.  
 1719 S. Grand Ave.  
 Santa Ana, CA 92705, USA  
 sales@btcpower.com  
 www.btcpower.com

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**eMOBILITY SOLUTIONS**

**eBox professional Data sheet**

**innogy**

**General and electrical specifications**

Specification	eBox professional
Charging power	up to 10.4kW (WYE) / up to 12kW (split phase/center tap delta) (up to 50A)
Applications	Protected internal areas; unprotected exterior areas exposed to rain and direct sunlight
Enclosure Rating	Type 3S
Operating temperature	-22°F to 122°F (-30°C to +50°C) full load
Storage temperature	-22°F to +176°F (-30°C to +80°C)
Air humidity	5% to 95% as defined under IEC 61851-1 Ed.3/EN 61851-1 (2017)
Max altitude above sea level	Max 6,500ft (2,000m) (air pressure: 860hPa to 1065hPa)
UV protection	Outdoors (F1)
Housing material	Lexan® EXL9330 (copolymer)
Protection category (impact strength)	Flame class rating V0 (UL94)
Weight (without eClick)	IK10 as defined under IEC 62262-2:2002
Number of charging ports	6, 4, 2
Plug assembly	Type 1 plug as defined under SAE J1772™, plug compartments on eBox, shutter
Cable length	24'-7 1/4" (7.5m)
Backend protocol	eOperate using LG2WAN for business applications and eHome using OCPP 2.0 for residential applications
Input power from eClick	up to 10.4kW (WYE) / up to 12kW (split phase / center tap delta) (up to 50A)
Output power	up to 10.4kW (WYE) / up to 12kW (split phase / center tap delta) (up to 50A)
Electrical Protection Class	I
Standby power	6 W
Charging mode	Mode 3 (IEC 61851)
Vehicle communication	Charging current controlled via PWM pilot signal (IEC 61851-1:2017)
Alternative vehicle communication	ISO 15118 ready via PLC
User protection: integrated	Integrated AC/DC sensitive GFCI, tripping at: 20 mA rms for AC and for DC
Integrated overvoltage protection	according to UL2231 (ESD/Surge/Burst)
Interaction	LED ring for charging status; 2 status LEDs: authentication / RFID, vehicle Id; 1 status LED as touch button: Bluetooth
Measured consumption	Current and voltage measured by eBox, power and energy provided with 99% accuracy
Direct communication	Bluetooth Class 1 and 2 (power level)
Backend link	eHome (eCharge) app: WLAN with 2.4 GHz b/g/n with WPA2 (saerl gain, frequency-dependent, max 4.6 dB) or LAN or eOperate: SIM-card (frequency- and direction-dependent, max 4.4 dB (saerl gain))
Authentication / activation	Free charging: eCharge+ app (smartphone app for iOS / Android™) from contracted providers or Direct Payment (credit card / PayPal), Direct Payment via web access
Plug/Charge (ISO 15118)	Yes
RFID authentication	Yes, protocols: MIFARE Classic (card and chip) as defined under ISO 14443A, Type V (ISO/IEC 15693/Vicinity)
Charging port number	Charge port number lasered on shutter

Remark: Android is a trademark of Google LLC.

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**eMOBILITY SOLUTIONS**

**eBox professional Data sheet**

**innogy**

The eBox professional is a modern, leading-edge charging solution for electric vehicles. It can be used easily and quickly for charging up to 12kW of power. It presents a large number of ports and authentication options. The eBoxes can be mounted in the eClick on a wall. The eBox professional is available with a Type 1 plug and cable.

**NOT USED**

(170 mm)

with Type 1 plug and cable

**Key features**

- Charging (up to 12 kW)
- Charging status displayed by 3 LEDs and LED ring
- Facilities for authenticating and paying for charging processes by app, RFID card or ISO 15118
- LTE / 4G / 2G communication with eOperate or WLAN, LAN with eCharge+ app
- Commissioning: simply clicking the eBox on the eClick and connect via Bluetooth with the smartphone

**Certificates**

UL US      FC

**VOLTA V4 EVCS CUT SHEET**      SCALE N.T.S.      **19**

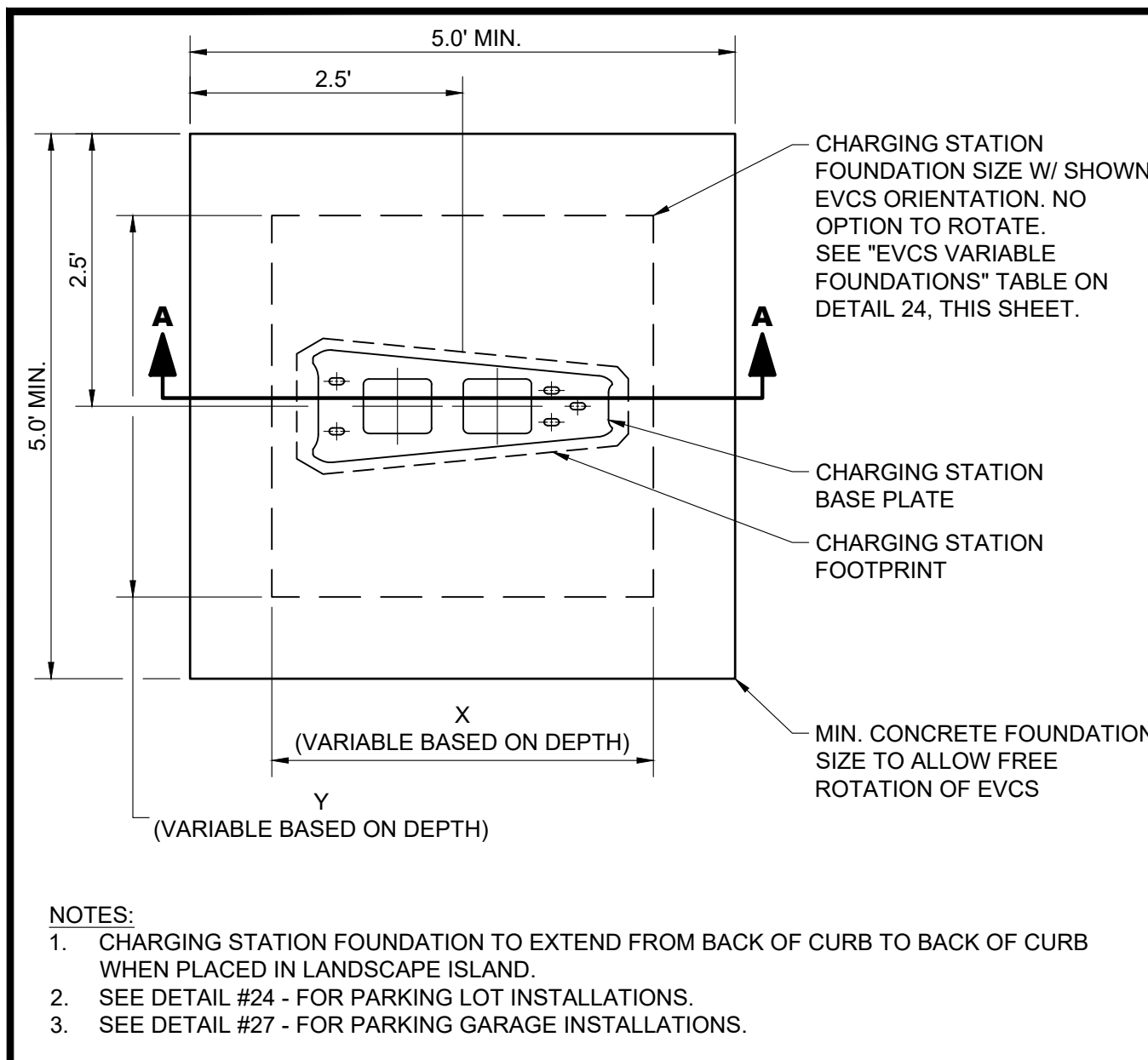
**ECLICK DATA CUT SHEET**      SCALE N.T.S.      **20**

**EBOX DATA CUT SHEET**      SCALE N.T.S.      **21**

**EBOX DATA CUT SHEET**      SCALE N.T.S.      **22**

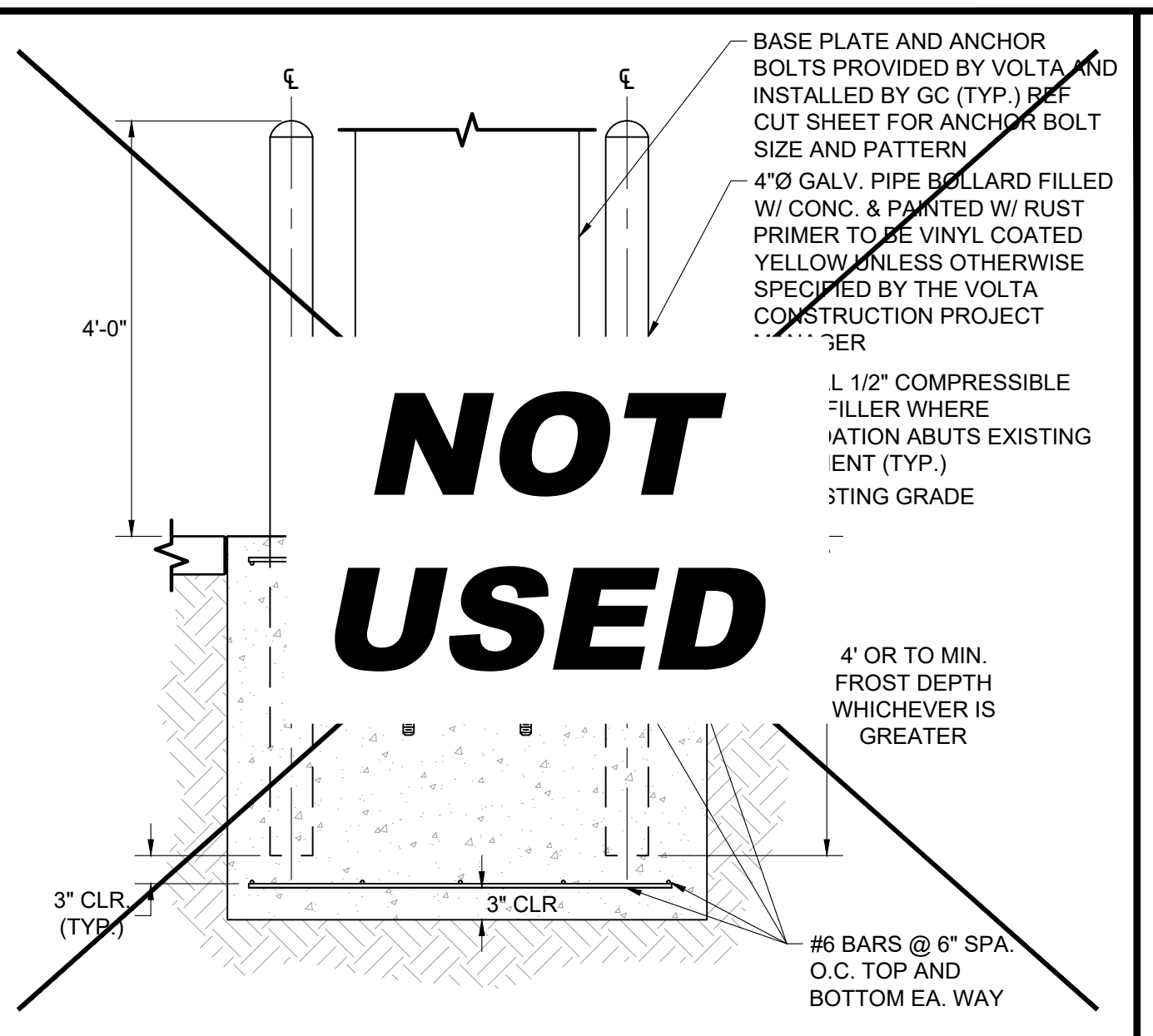
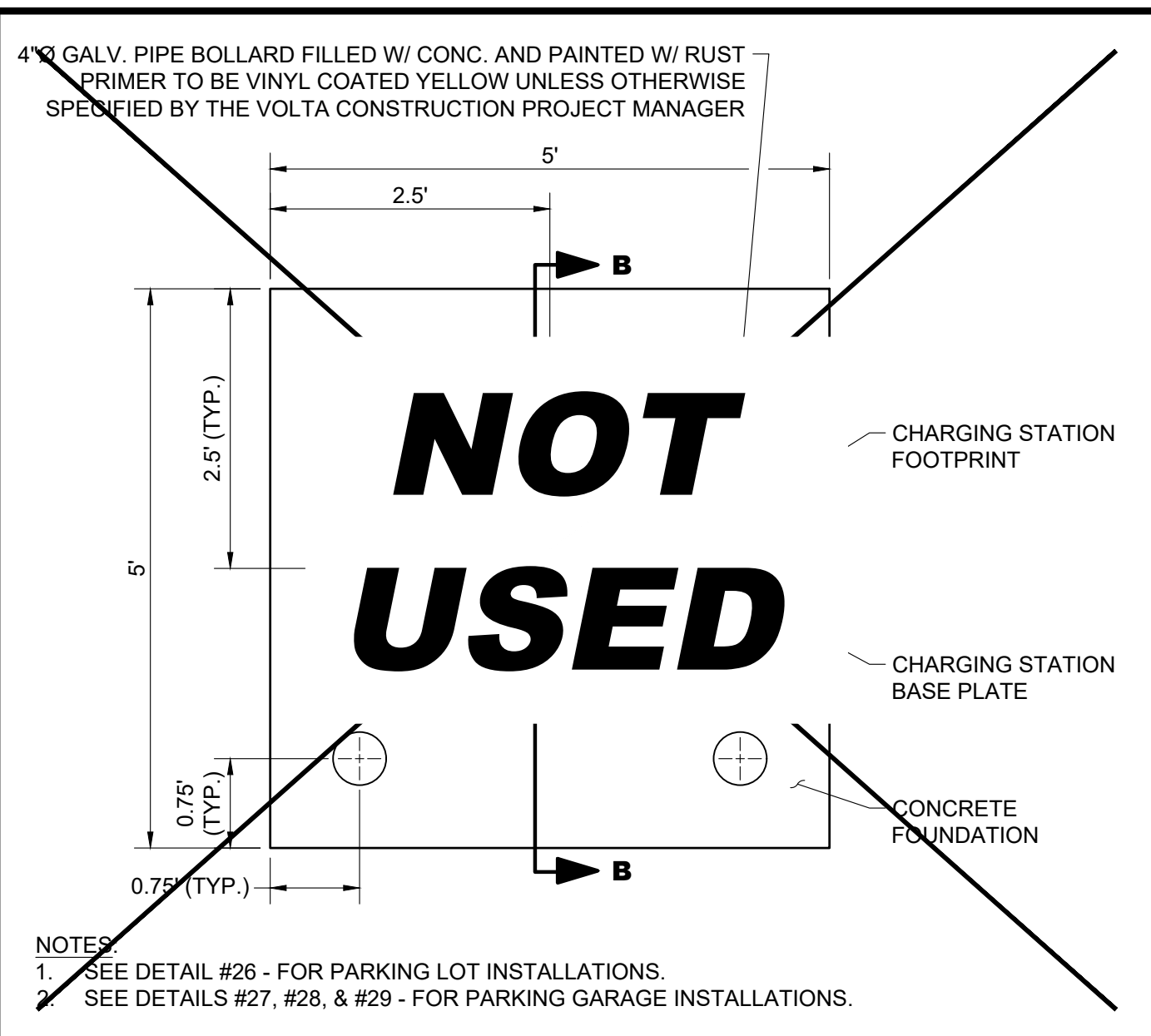
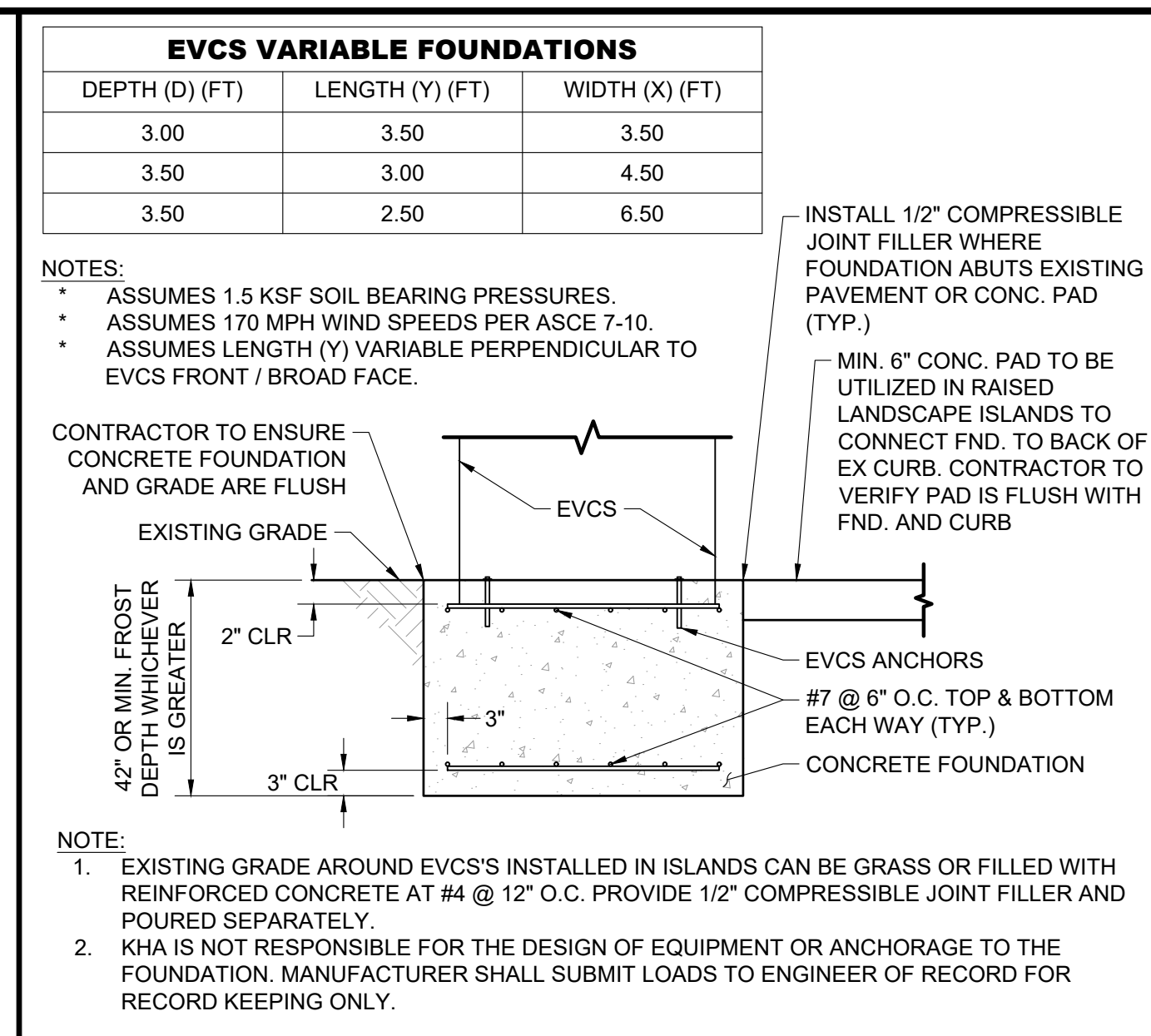
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NOTES:

- CHARGING STATION FOUNDATION TO EXTEND FROM BACK OF CURB TO BACK OF CURB WHEN PLACED IN LANDSCAPE ISLAND.
- SEE DETAIL #24 - FOR PARKING LOT INSTALLATIONS.
- SEE DETAIL #27 - FOR PARKING GARAGE INSTALLATIONS.

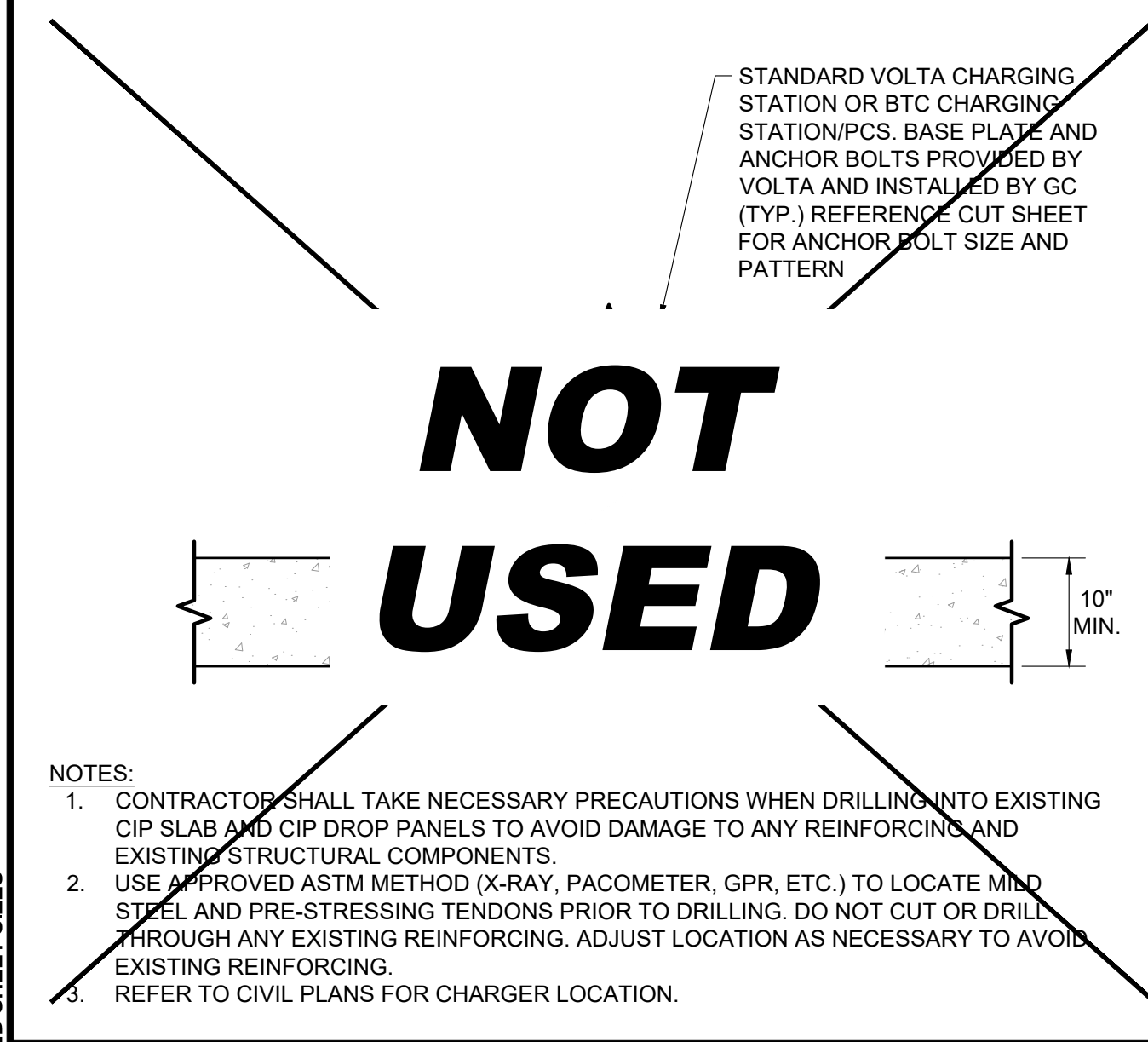


V4 EVCS FOUNDATION SCALE N.T.S. 23

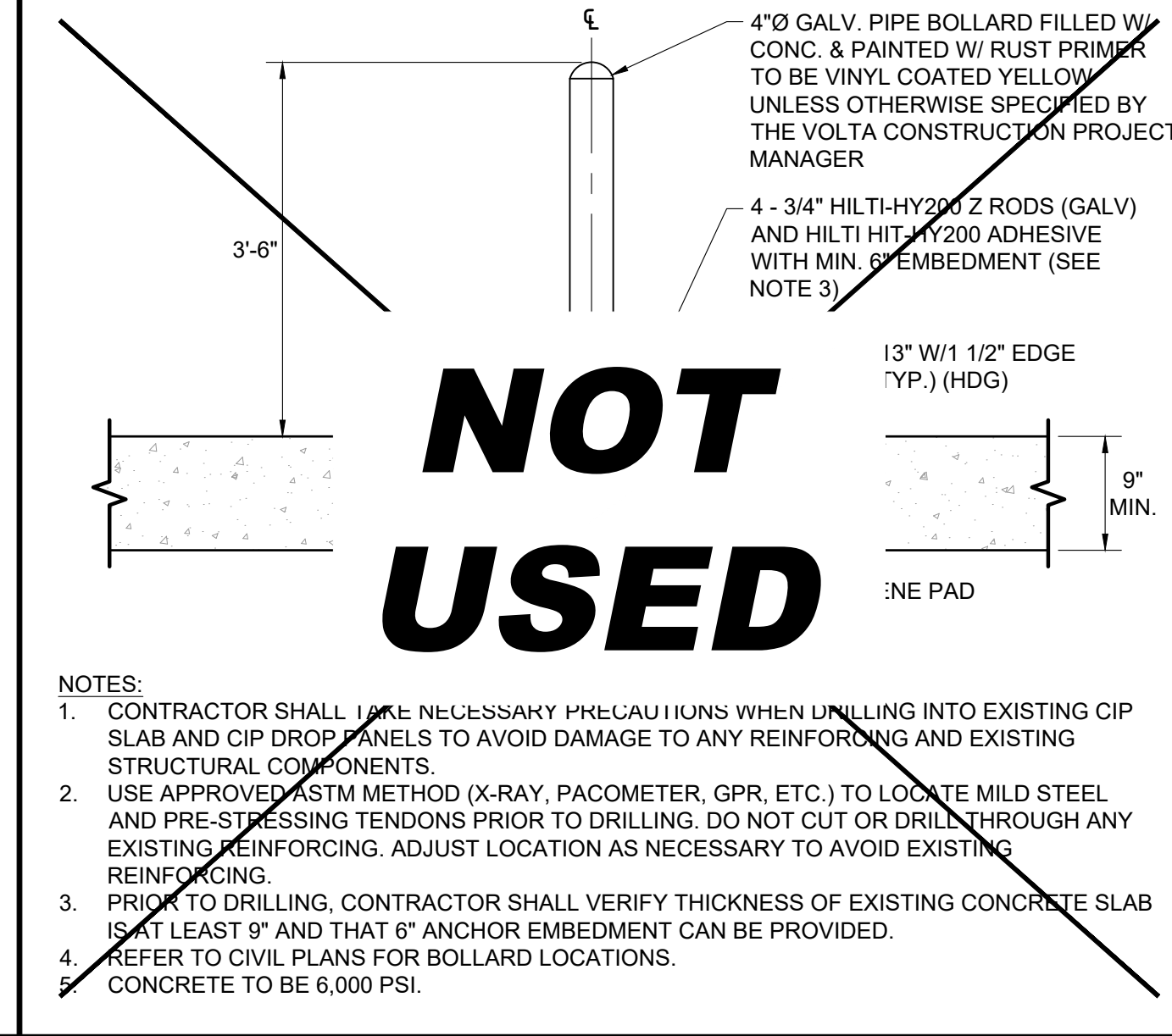
V4 EVCS FOUNDATION SECTION A-A SCALE N.T.S. 24

V4 EVCS FOUNDATION W/ 4" PIPE BOL. SCALE N.T.S. 25

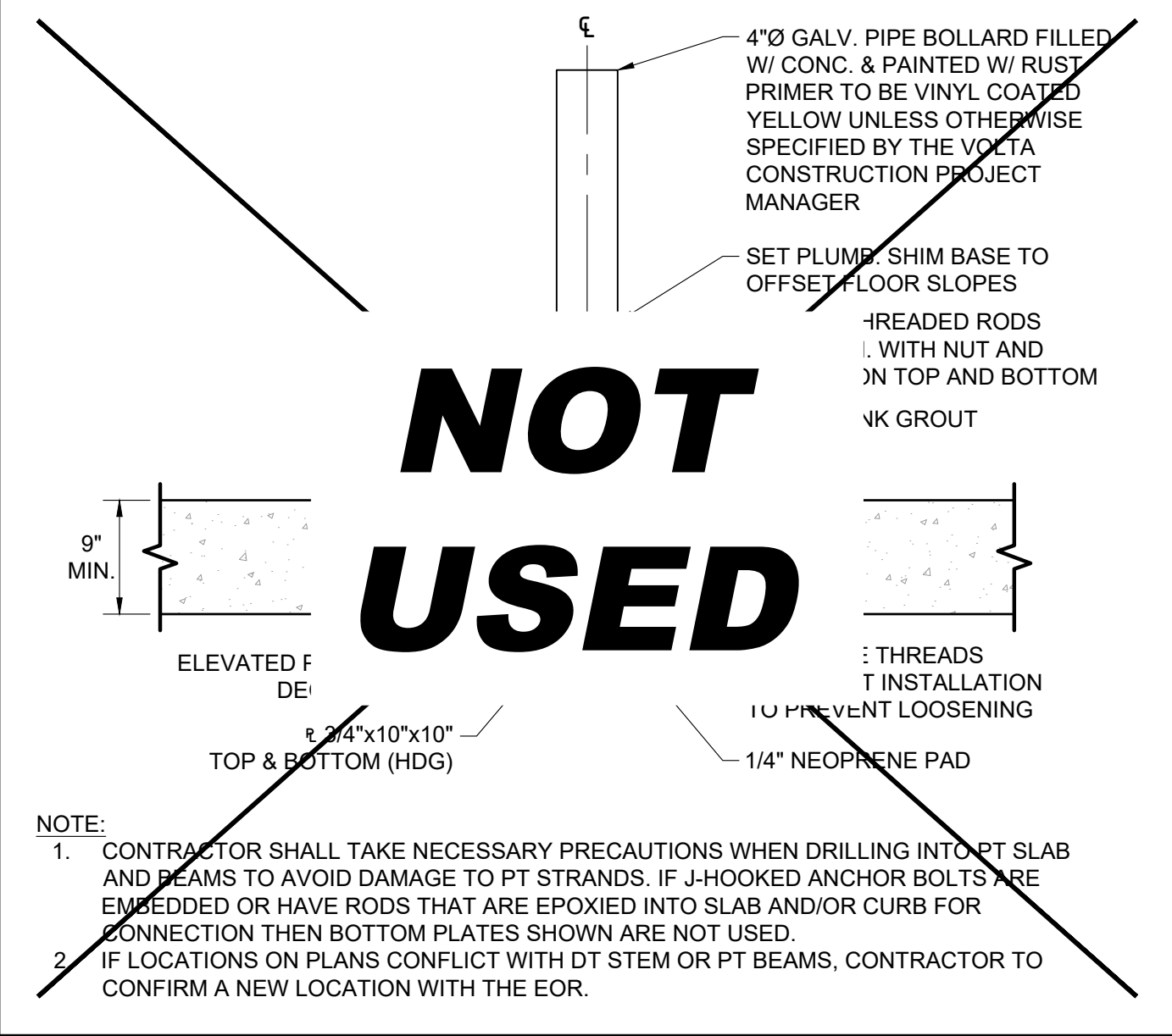
EVCS FOUNDATION SECTION B-B SCALE N.T.S. 26



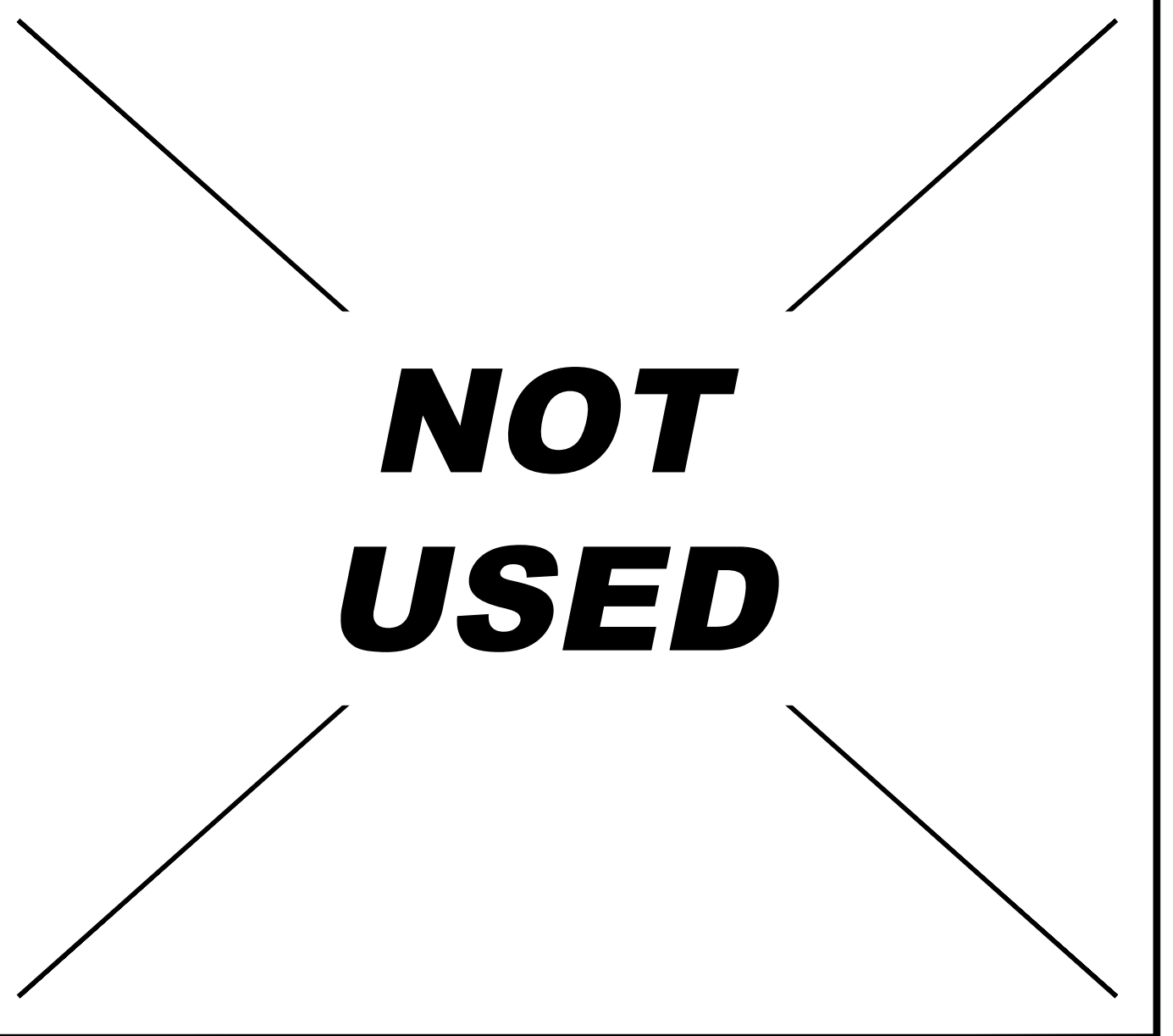
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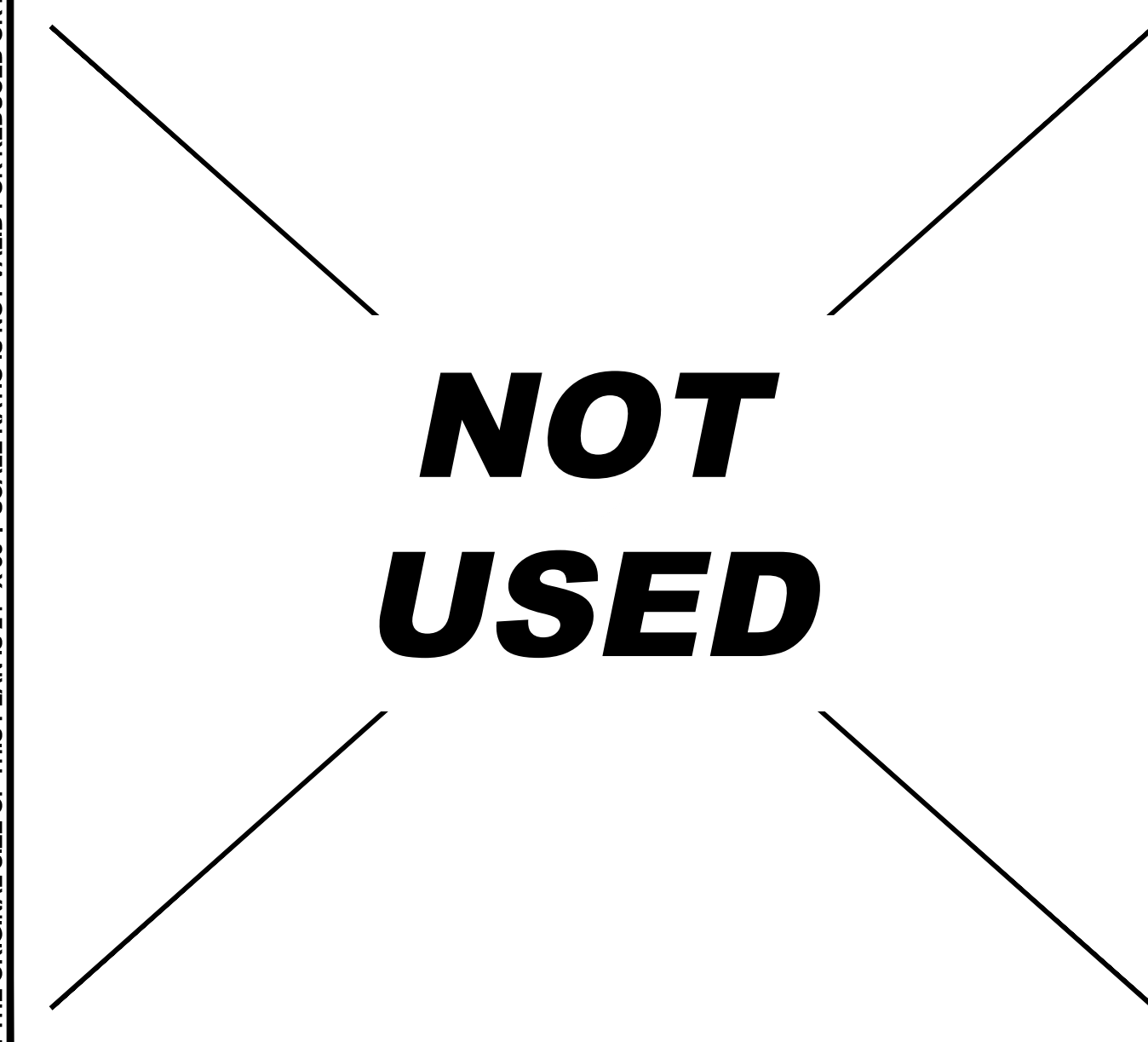
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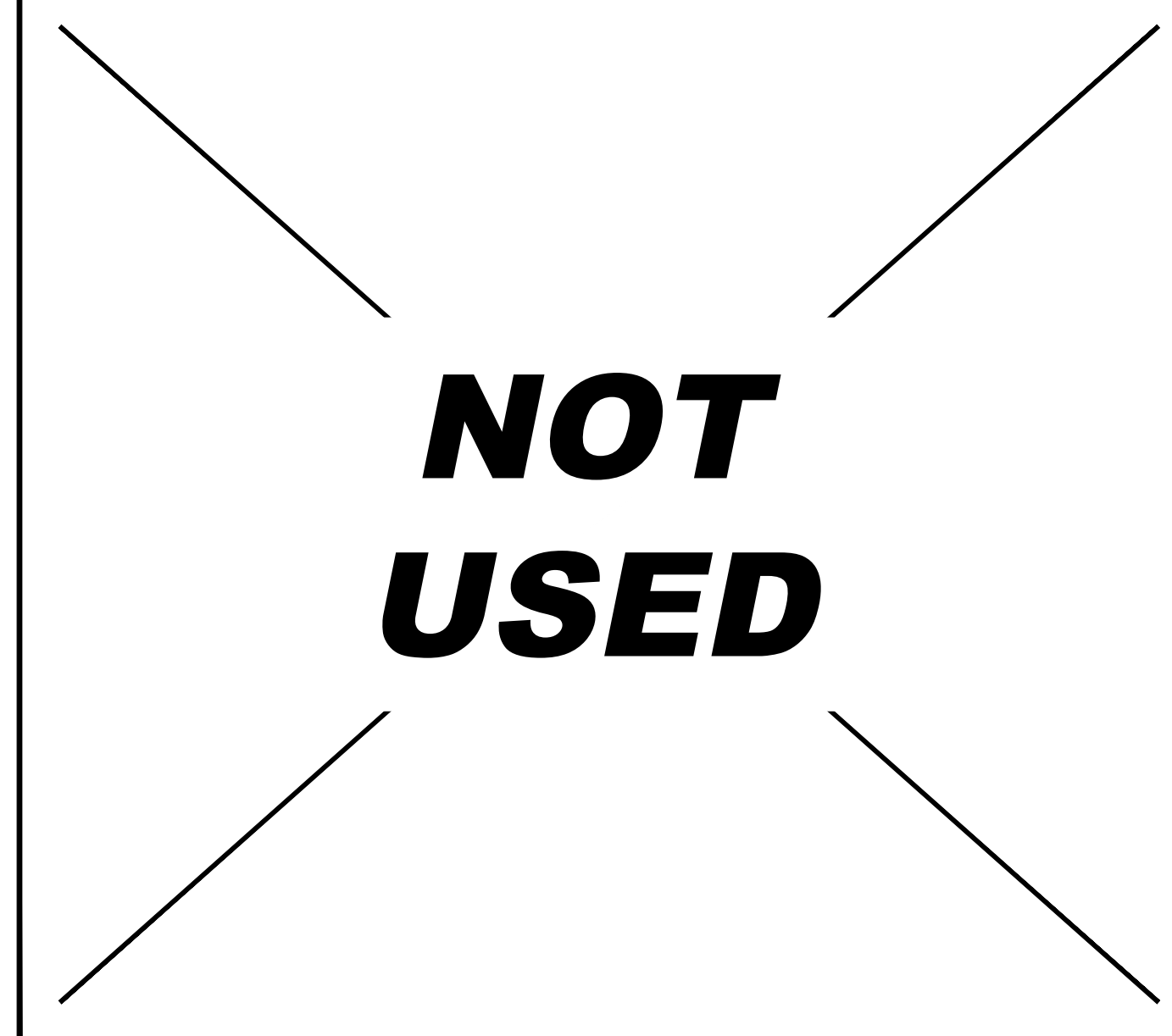
BOLLARD CONNECTION-SLAB MOUNT SCALE N.T.S. 29



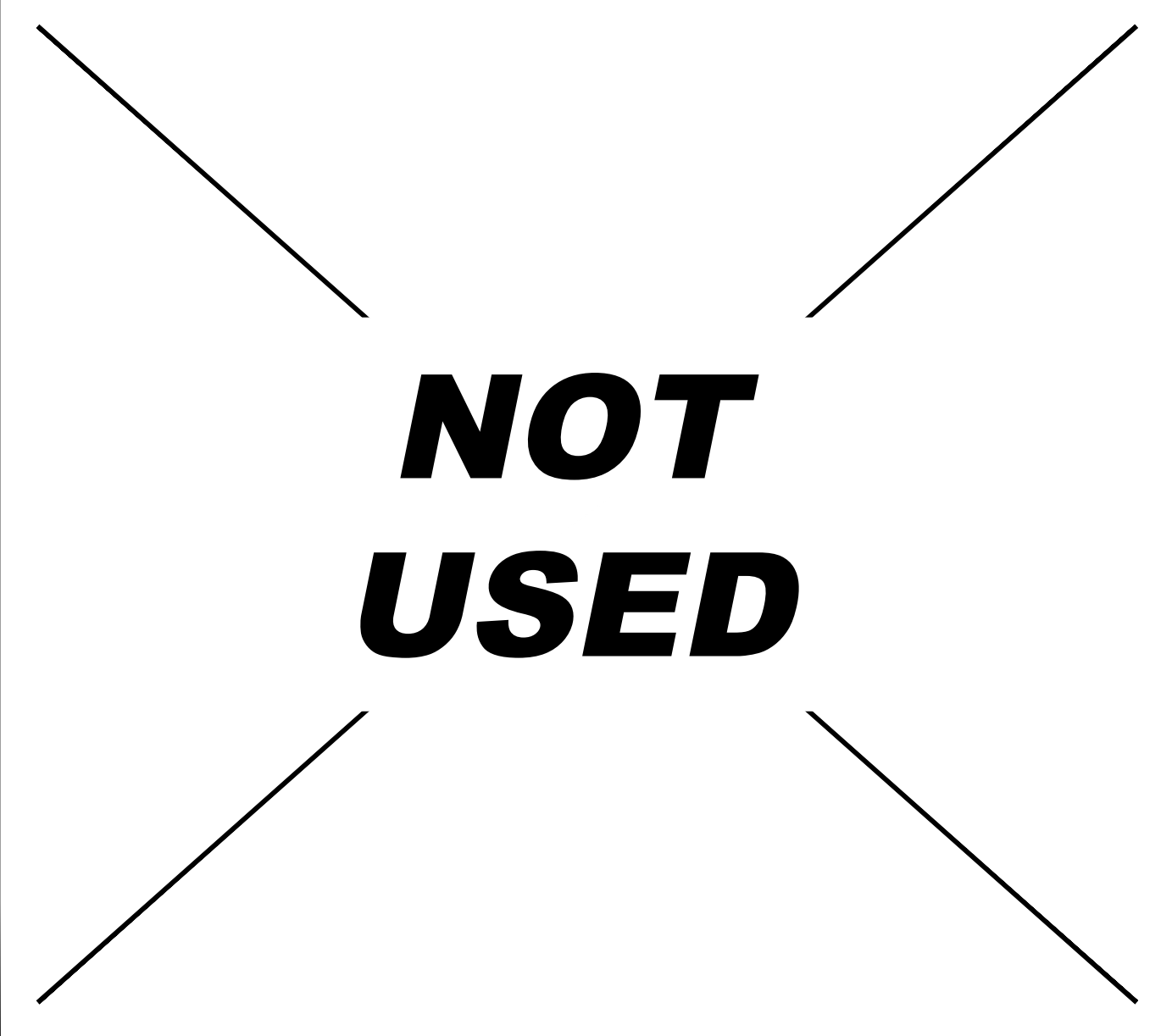
NOT USED SCALE N.T.S. 30



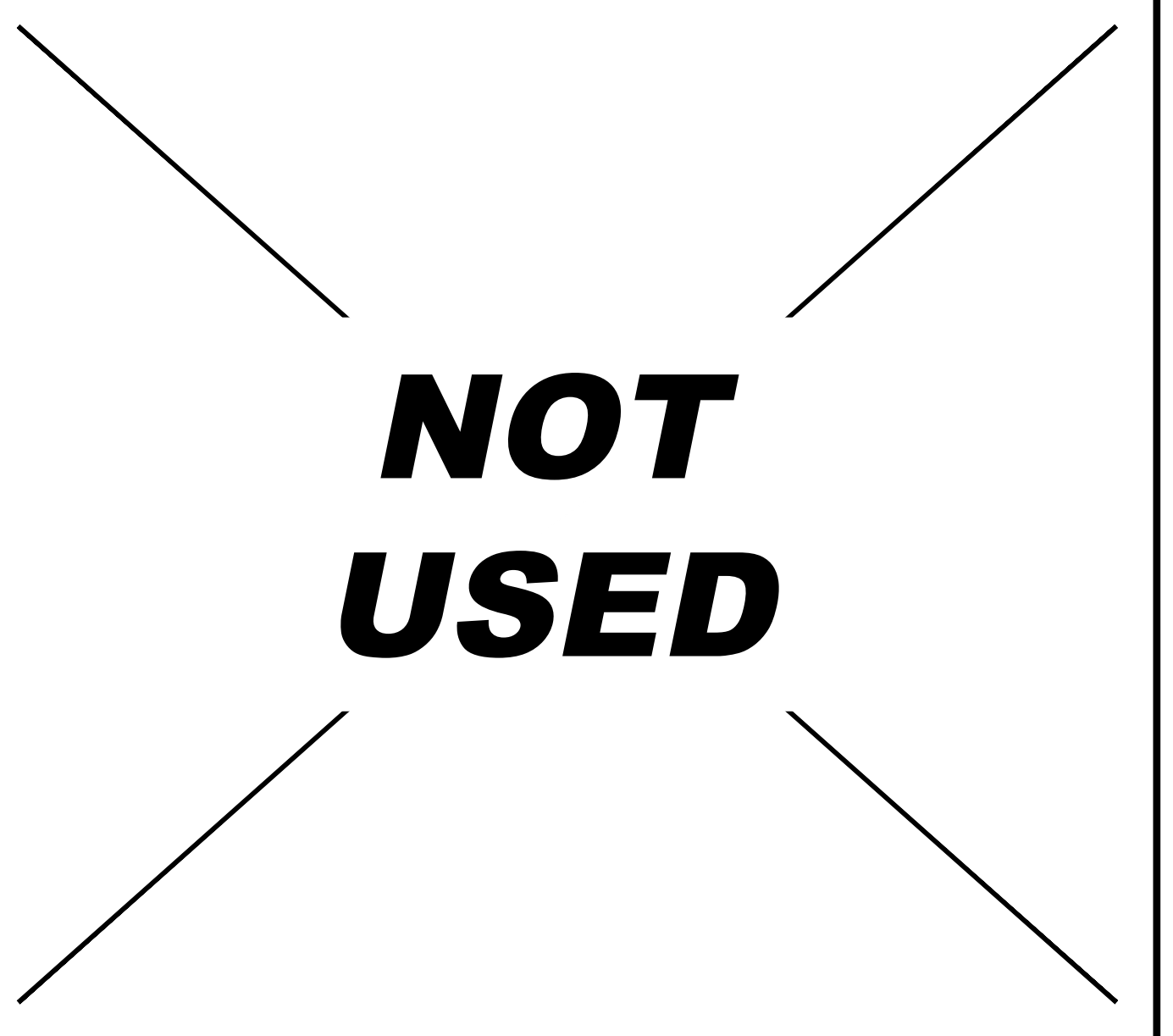
NOT USED SCALE N.T.S. 31



NOT USED SCALE N.T.S. 32



NOT USED SCALE N.T.S. 33



NOT USED SCALE N.T.S. 34

**volta**

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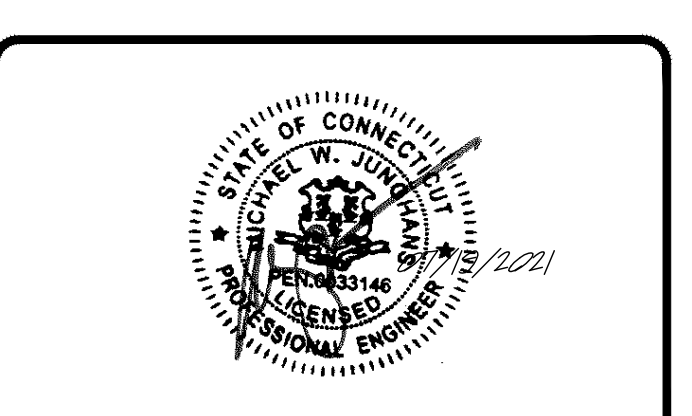
**Kimley Horn**

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REV	DATE	DESCRIPTION	BY
1	04/06/2021	CD90S	UK
2	06/04/2021	CD100	VRP
3	07/23/2021	CD100S REVISIONS PER VOLTA COMMENTS	VRP

ISSUE DATE  
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**STOP AND SHOP #683**

180 DANBURY ROAD  
NEW MILFORD, CT 06776

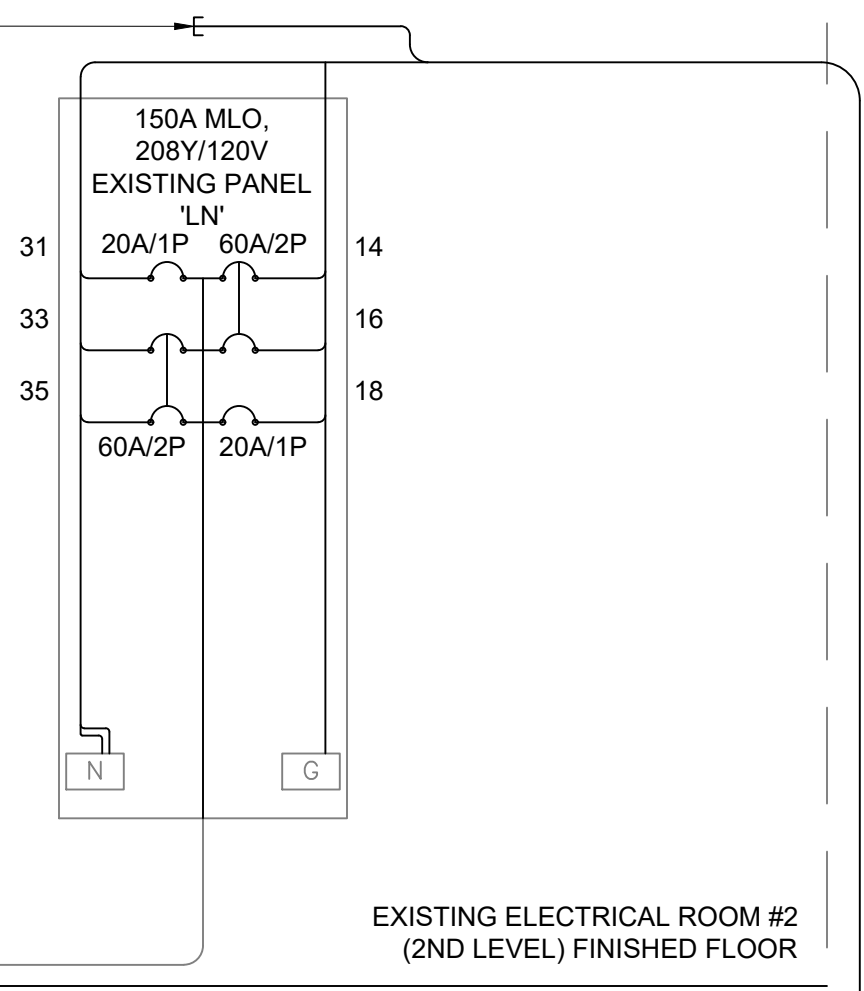
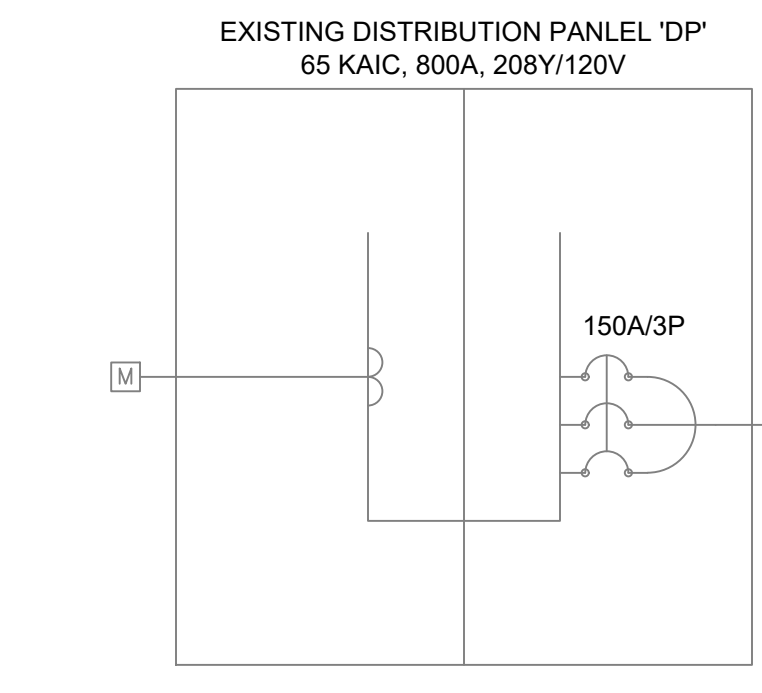
SHEET TITLE  
**SITE DETAILS**

SHEET NUMBER  
**C3-03**

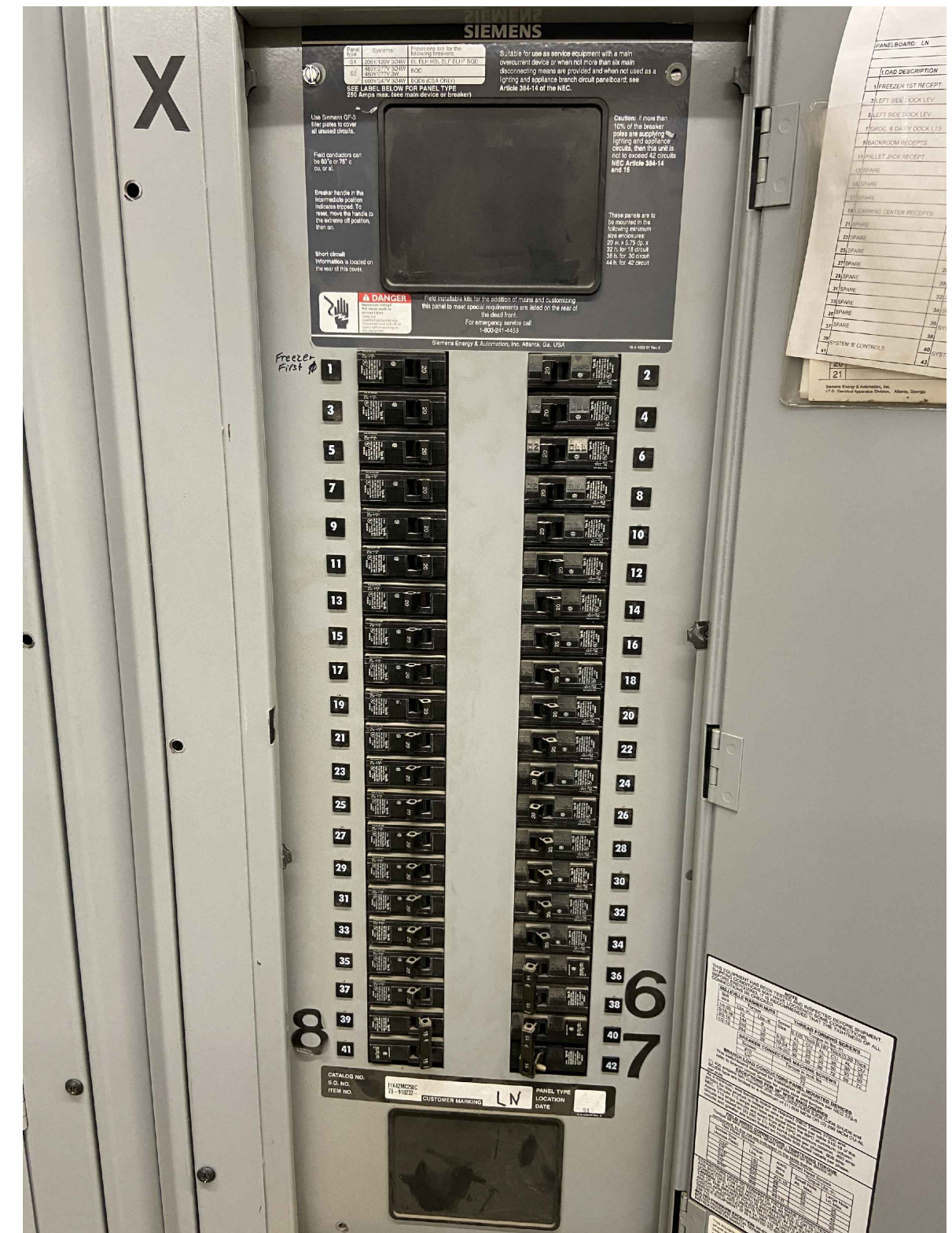
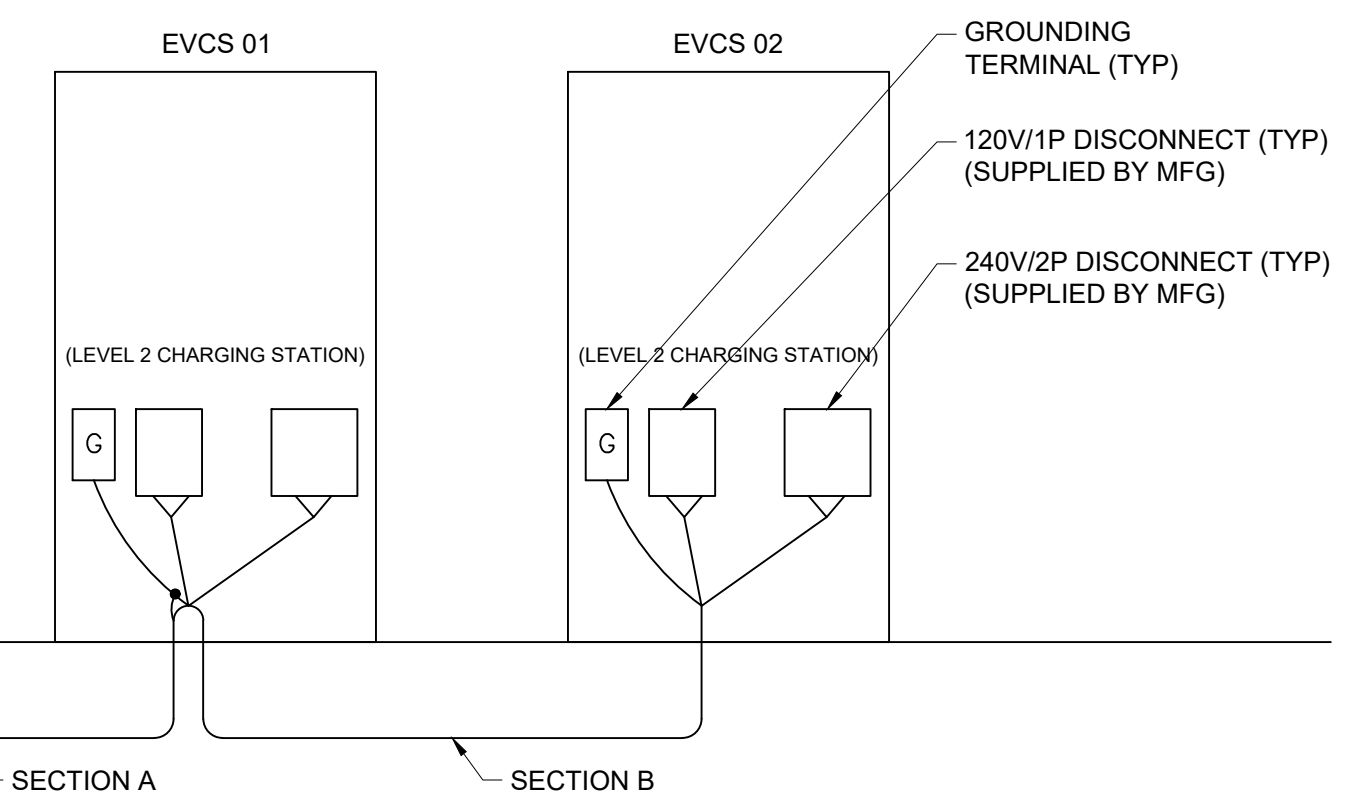
NOTE: THE ORIGINAL SIZE OF THIS PLAN IS 24" X 36". SCALE RATIO IS NOT VALID FOR REDUCED OR ENLARGED SHEET SIZES



CONDUIT FOR FUTURE COMMUNICATIONS. SEAL AND CAP. LABEL CONDUIT "VOLTA COMMUNICATION CONDUIT" WITH PERMANENT LABELING MATERIAL.



EXISTING ELECTRICAL ROOM #2 (2ND LEVEL) FINISHED FLOOR



EXISTING ELECTRICAL PANEL 'LN'

- NOTES:**
- ALL ELECTRICAL WORK AND RELATED ACTIVITIES PERFORMED ON SITE SHALL BE DONE IN ACCORDANCE WITH NATIONAL ELECTRICAL CODE (NEC) STANDARDS BEING ENFORCED BY ALL APPLICABLE JURISDICTIONAL REQUIREMENTS AT THE TIME OF CONSTRUCTION.
  - ANY PAVEMENT DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITIONS OR BETTER.
  - CONTRACTOR SHALL USE THWN COPPER CONDUCTORS.
  - CONTRACTOR SHALL USE EMT INSIDE AND OUTSIDE ABOVE GRADE WHERE NOT SUBJECT TO DAMAGE. CONTRACTOR SHALL RGS INSIDE AND OUTSIDE ABOVE GRADE WHERE SUBJECT TO DAMAGE. CONTRACTOR SHALL USE PVC SCHEDULE 80 BELOW GRADE.
  - SEE SHEETS C1-00 AND C2-00 FOR CONDUIT STUB UP LOCATIONS.
  - CONTRACTOR TO LOCATE JUNCTION BOX, LINE BOX (LB), OR APPROVED ALTERNATIVE FOR SITE SPECIFIC RUN LENGTHS AND BENDS.
  - CONTRACTOR TO RELOCATE CIRCUIT #3 TO CIRCUIT #13 AND TO RELOCATE CIRCUIT #4 TO CIRCUIT #20 ON PANEL 'LN'. NOTIFY ENGINEER IMMEDIATELY IF CIRCUITS MARKED AS SPARES ARE IN USE.

Panel Schedule															
Existing Panel 'LN' Location: Existing Electrical Room #2 (2nd Level) Vols: 208Y/120V Phase: 3 Wire: 4 Hertz: 60															
150A MLO Main AIC: N/A Branch AIC: (See Note 3) ENCL. (NEMA): 1 MTG: Surface															
225 Amp Frame - Ground Bar, Locking Cover, Panel Card															
Description of Load Served	Breaker		Wire	A/Phase			CKT No.	CKT No.	A/Phase			Wire	Breaker		Description of Load Served
	Amp	Pole		A	B	C			A	B	C		Amp	Pole	
FREEZER 1ST RECEP	20	1	EXIST	1.5	-	-	1	2	1.5	-	-	EXIST	20	1	ROOFTOP SERV. RECEP
SPARE	20	1					3	4				EXIST	20	1	SPARE
LEFT SIDE DOCK LEV.	20	1	EXIST			10.0	5	6			10.0	EXIST	20	1	PHARMACY SIGN
GROC. & DAIRY DOCK LTS	20	1	EXIST	10.0			7	8	10.0			EXIST	20	1	PHARMACY SIGN
BACKROOM RECEP	20	1	EXIST		1.5		9	10				EXIST	20	1	SPARE
PALLET JACK RECEP	20	1	EXIST			1.5	11	12				EXIST	20	1	SPARE
LEFT SIDE DOCK LEV.	20	1	EXIST	10.0			13	14	48.0			See Note 5	60	2	CHARGING STATION EV02
SPARE	20	1					15	16		48.0		See Note 5	20	1	CHARGING STATION EV02
SPARE	20	1					17	18			5.0	See Note 5	20	1	CHARGING STATION EV02
LEARNING CENTER RECEP	20	1	EXIST	1.5			19	20	10.0			EXIST	20	1	EXISTING LOAD
SPARE	20	1					21	22				EXIST	20	1	SPARE
SPARE	20	1					23	24			10.0	EXIST	20	1	EXISTING LOAD
SPARE	20	1					25	26	10.0			EXIST	20	1	EXISTING LOAD
SPARE	20	1					27	28				EXIST	20	1	SPARE
SPARE	20	1					29	30				EXIST	20	1	SPARE
CHARGING STATION EV01	20	1	See Note 5	5.0			31	32				EXIST	20	1	SPARE
CHARGING STATION EV01	60	2	See Note 5		48.0		33	34				EXIST	20	1	SPARE
SPARE	20	1				48.0	35	36				EXIST	15	2	SYSTEM 'A' CONTROLS
SPARE	20	1					37	38	10.0			EXIST	15	2	SYSTEM 'A' CONTROLS
SYSTEM 'B' CONTROLS	15	2	EXIST		10.0		39	40		10.0		EXIST	15	2	SYSTEM 'C' CONTROLS
					10.0		41	42			10.0	EXIST	15	2	SYSTEM 'C' CONTROLS
					28.0	59.5	69.5			89.5	58.0	45.0			Total A/Phase

Notes:  
 1. Connected KVA (New): 41.9  
 2. Demand KVA (New): 52.4  
 3. Contractor shall match existing AIC Rating.  
 4. Where load is labeled "EX" the load is unknown.  
 5. See Voltage Drop Table for conductor sizing.

L2 Conductor Voltage Drop Table Per Charging Station										
<175FT	175FT-200FT	200FT-255FT	255FT-275FT	275FT-320FT	320FT-400FT	400FT-440FT	440FT-510FT	510FT-635FT	635FT-700FT	700FT-800FT
(2) #4 AWG +	(2) #4 AWG +	(2) #3 AWG +	(2) #2 AWG +	(2) #2 AWG +	(2) #1 AWG +	(2) #1/0 AWG +	(2) #1/0 AWG +	(2) #2/0 AWG +	(2) #3/0 AWG +	(2) #3/0 AWG +
(2) #12 AWG +	(2) #10 AWG +	(2) #10 AWG +	(2) #10 AWG +	(2) #8 AWG +	(2) #8 AWG +	(2) #8 AWG +	(2) #6 AWG +	(2) #6 AWG +	(2) #6 AWG +	(2) #4 AWG +
(1) #6 AWG GND	(1) #6 AWG GND	(1) #4 AWG GND	(1) #4 AWG GND	(1) #4 AWG GND	(1) #4 AWG GND	(1) #3 AWG GND	(1) #3 AWG GND	(1) #2 AWG GND	(1) #1 AWG GND	(1) #1 AWG GND

- VOLTAGE DROP TABLE NOTES**
- CONTRACTOR SHALL BE RESPONSIBLE FOR DE-RATING CONDUCTORS WHEN 4 OR MORE CURRENT CARRYING CONDUCTORS ARE CARRIED IN THE SAME CONDUIT PER THE NEC.
  - THE DISTANCES IN THIS TABLE ARE TOTAL DISTANCES, NOT HORIZONTAL DISTANCES. INCLUDE VERTICAL RUNS AND JUNCTION BOX COIL LENGTH IN THE TOTAL CONDUCTOR DISTANCE.
  - WHEN MORE THAN ONE CHARGING STATION CIRCUIT CONDUCTORS ARE IN A CONDUIT, USE ONLY ONE SHARED EQUIPMENT GROUND CONDUCTOR.
  - WHEN THERE IS A REMOTE HOLSTER, INSTALL (2) #6 AWG + (1) #8 AWG GND + (1) #14 AWG COMM IN THE CONDUIT FROM EVCS TO REMOTE HOLSTER.
  - WHEN INSTALLING #1/0 AWG OR LARGER CONDUCTORS FROM THE POWER SOURCE TO EVCS, INCLUDE MULTICONDUCTOR TAPS IN THE CLOSEST JUNCTION BOX PRIOR TO ENTERING THE EVCS OR IN THE EVCS ITSELF SO THAT #6 AWG CONDUCTORS CAN BE TERMINATED IN THE EVCS.

Conduit Schedule			
Conduit Section	Conduit #	Conduit Size	Installation Method
A	1	2"	Interior Mount / Surface Mount / Directional Bore
	2	3/4" Min	
B	1	2"	Directional Bore
	2	3/4" Min	

NOTE: THE ORIGINAL SIZE OF THIS PLAN IS 24" X 36". SCALE RATIO IS NOT VALID FOR REDUCED OR ENLARGED SHEET SIZES

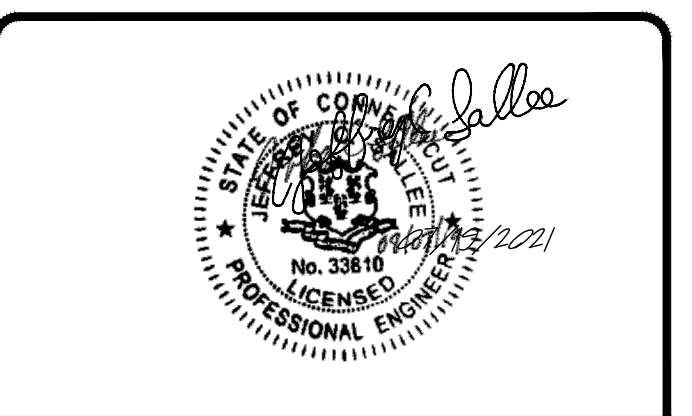
**volta**  
 155 DE HARO STREET  
 SAN FRANCISCO, CA 94103

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**STOP AND SHOP #683**  
 180 DANBURY ROAD  
 NEW MILFORD, CT 06776

SHEET TITLE  
**ELECTRICAL ONE LINE DIAGRAM & PANEL SCHEDULE**

SHEET NUMBER  
**E1-00**



**NOTES:**

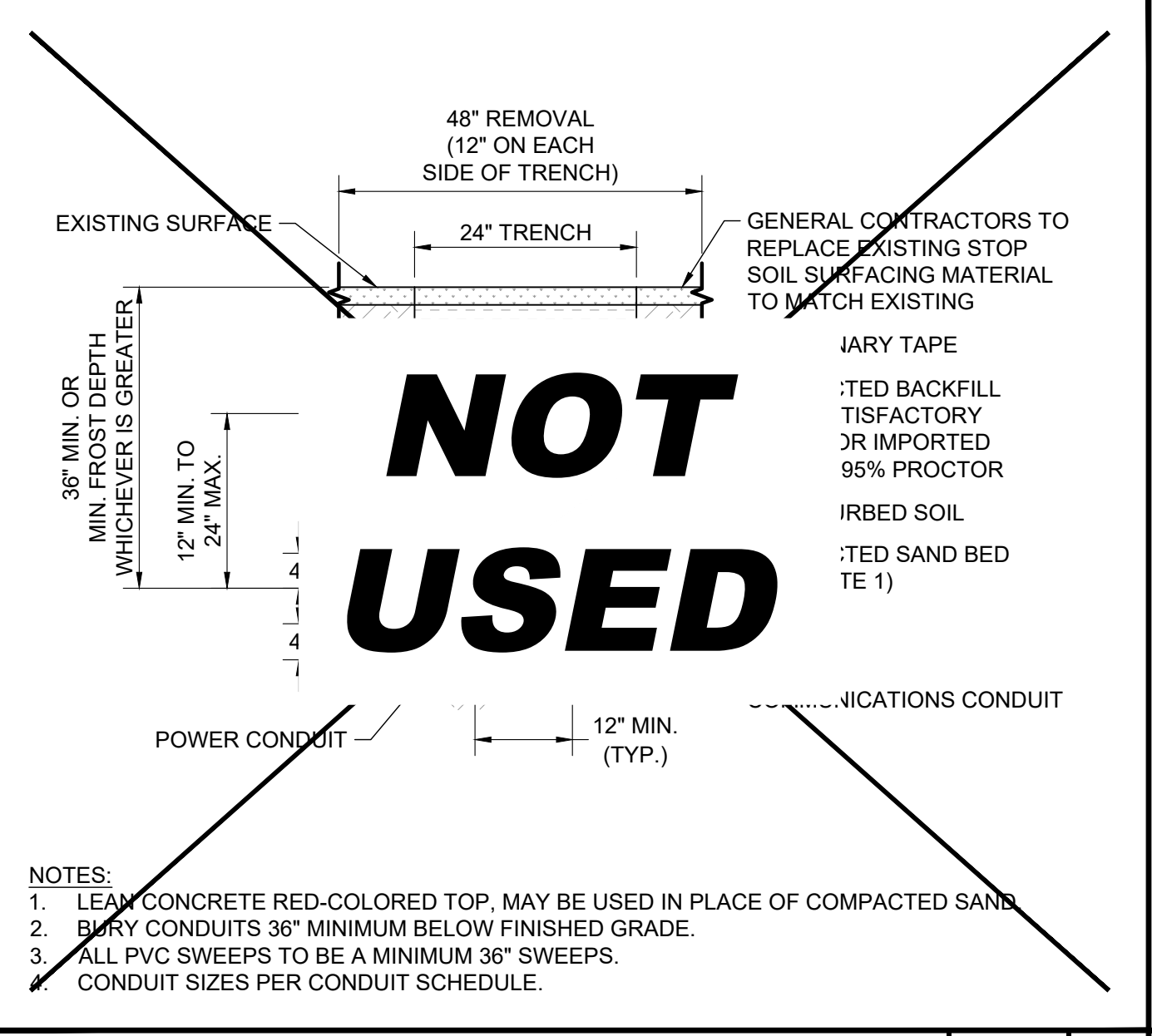
1. A NATIONALLY RECOGNIZED TESTING LABORATORY SHALL LIST ALL EQUIPMENT IN COMPLIANCE WITH ART110.3.
2. ALL EXTERIOR EQUIPMENT SHALL BE RAIN TIGHT AND APPROVED FOR USE IN WET CONDITIONS.
3. ALL CONDUCTORS SHALL BE PROVIDED WITH STRAIN RELIEF UPON ENTRY INTO ENCLOSURES.
4. EACH UNGROUNDED CONDUCTOR SHALL BE IDENTIFIED BY PHASE AND SYSTEM PER ART 210.5.
5. ALL METALLIC COMPONENTS SHALL BE GROUNDED VIA EQUIPMENT GROUNDING CONDUCTORS.
6. CHARGING UNITS ARE EQUIPPED WITH AN INTEGRATED CONTRACTOR TO PREVENT BACK FEEDING OF POWER TO THE SOURCE.
7. CONTRACTOR TO FIELD VERIFY MAIN FEED BREAKER SUPPORTING DISTRIBUTION PANEL IS APPROPRIATELY SIZED TO SUPPORT THE LOAD. CONTRACTOR SHALL CONTACT THE ENGINEERING TEAM IMMEDIATELY IF BREAKER IS FOUND TO BE INSUFFICIENT.
8. CONTRACTOR SHALL INSPECT ALL PRE-WIRED CONNECTIONS WITHIN EACH CHARGING STATION TO ENSURE THE CONNECTIONS ARE SOLID. INFORM VOLTA OR THE ENGINEER IF ANY CONNECTIONS ARE LOOSE OR DAMAGED.

**ABBREVIATIONS:**

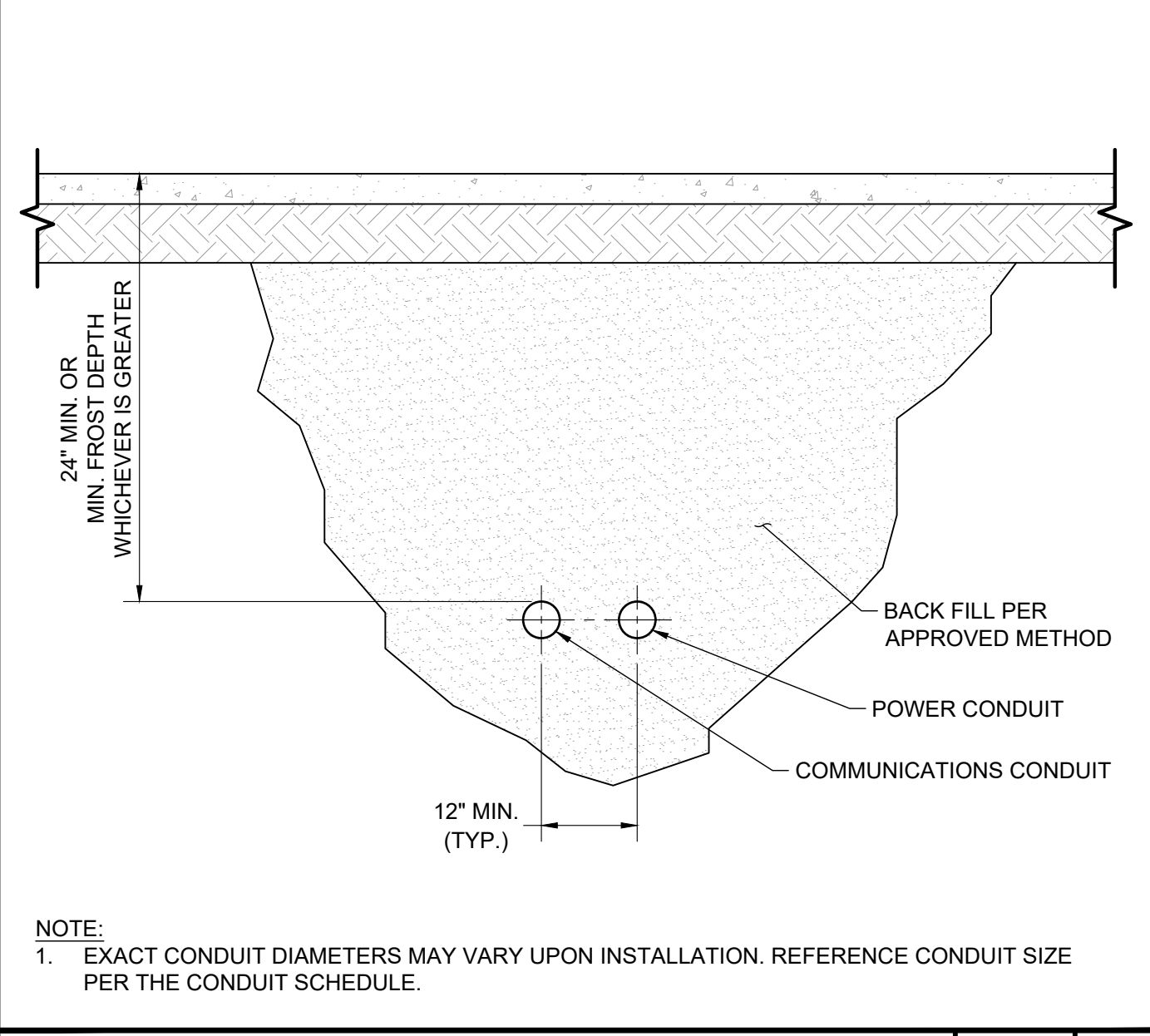
- A AMPERE
- AC ALTERNATING CURRENT
- AL ALUMINUM
- ART ARTICLE
- AUX AUXILIARY
- BLDG BUILDING STRUCTURE
- CONC CONCRETE
- CU COPPER
- DC DIRECT CURRENT
- EGC EQUIPMENT GROUNDING CONDUCTOR
- (E) EXISTING
- EMT ELECTRIC METALLIC TUBING
- EV ELECTRIC VEHICLE
- EVSE ELECTRIC VEHICLE SUPPLY EQUIPMENT
- GALV GALVANIZED
- GND GROUND
- HDG HOT DIPPED GALVANIZED
- I CURRENT
- KVA KILOVOLT AMPERE
- KW KILOWATT
- M METER
- MAX MAXIMUM
- MIN MINIMUM
- N NEUTRAL
- NEC NATIONAL ELECTRIC CODE
- NTS NOT TO SCALE
- (N) NEW
- OC ON CENTER
- PL PROPERTY LINE
- PVC POLYVINYL CHLORIDE
- RMC RIGID METALLIC CONDUIT
- SCH SCHEDULE
- SS STAINLESS STEEL
- TYP TYPICAL
- V VOLT
- W WATT
- XFMR TRANSFORMER

**ELECTRICAL NOTES:**

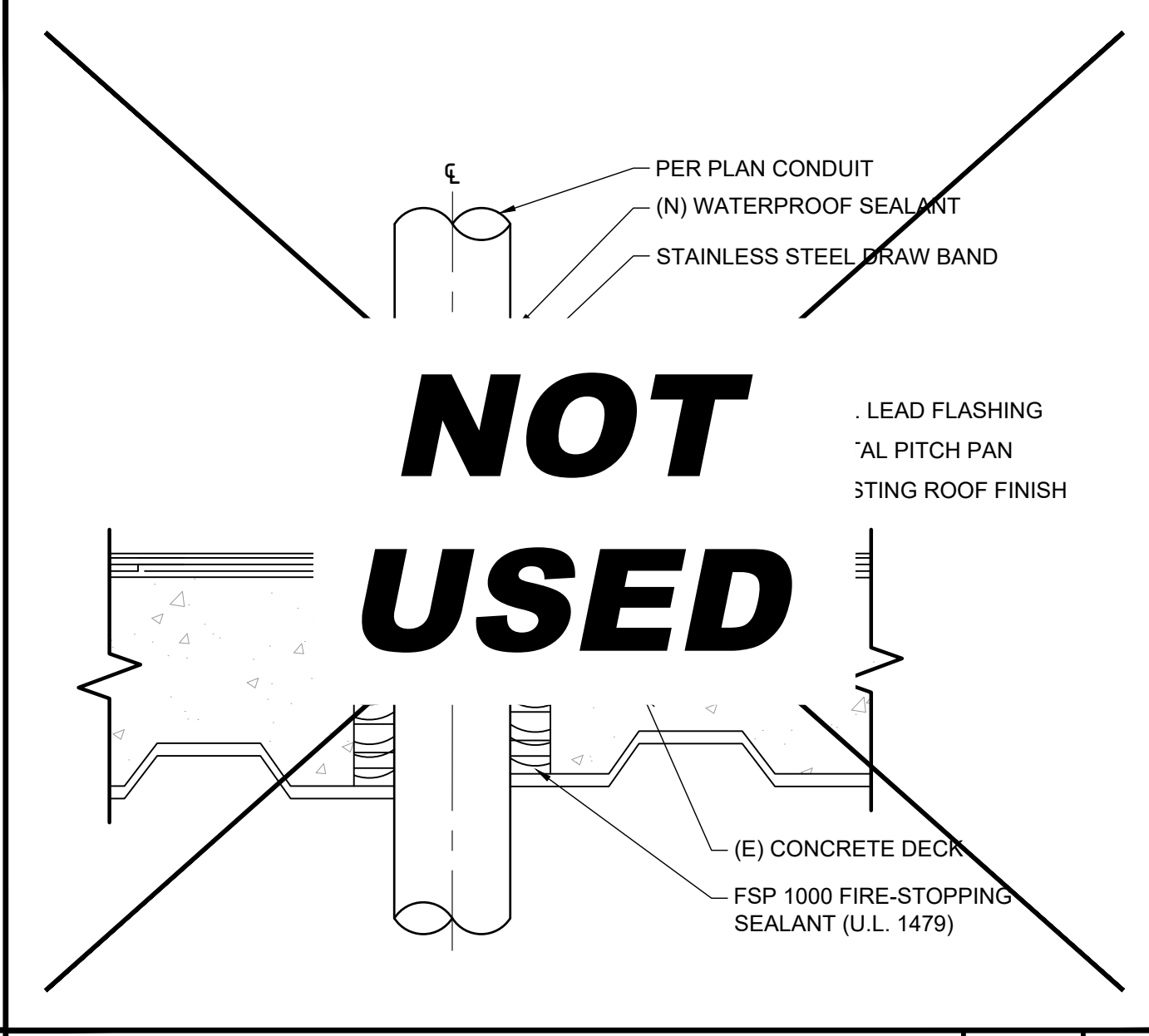
1. ALL ELECTRICAL WORK AND RELATED ACTIVITIES PERFORMED ON-SITE SHALL BE DONE IN ACCORDANCE WITH NATIONAL ELECTRIC CODE (NEC) STANDARDS BEING ENFORCED BY ALL APPLICABLE JURISDICTIONAL REQUIREMENTS AT THE TIME OF CONSTRUCTION.
2. UTILITY EQUIPMENT INSTALLATIONS AND PREP WORK SHALL BE COORDINATED WITH THE APPROPRIATE UTILITY ENGINEER AT TIME OF PRECONSTRUCTION MEETING TO ENSURE ACCURACY OF INSTALLATIONS.
3. CONDUIT PATHS ARE REPRESENTATIVE ONLY. EXACT CONDUIT PLACEMENT TO BE DETERMINED ON SITE BASED ON FIELD CONDITIONS.
4. CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS WHEN DRILLING INTO EXISTING CIP SLAB AND CIP DROP PANELS TO AVOID DAMAGE TO ANY REINFORCING AND EXISTING STRUCTURAL COMPONENTS.
5. USE APPROVED ASTM METHOD (X-RAY, PACOMETER, GPR, ETC.) TO LOCATE MILD STEEL AND PRE-STRESSING TENDONS PRIOR TO DRILLING. DO NOT CUT OR DRILL THROUGH ANY EXISTING REINFORCING. ADJUST LOCATION AS NECESSARY TO AVOID EXISTING REINFORCING.
6. PRIOR TO DRILLING, CONTRACTOR SHALL VERIFY THICKNESS OF EXISTING CONCRETE WALL IS AT LEAST 9" AND THAT 6" ANCHOR EMBEDMENT CAN BE PROVIDED.
7. REFER TO CIVIL PLANS FOR WALL MOUNT LOCATIONS.



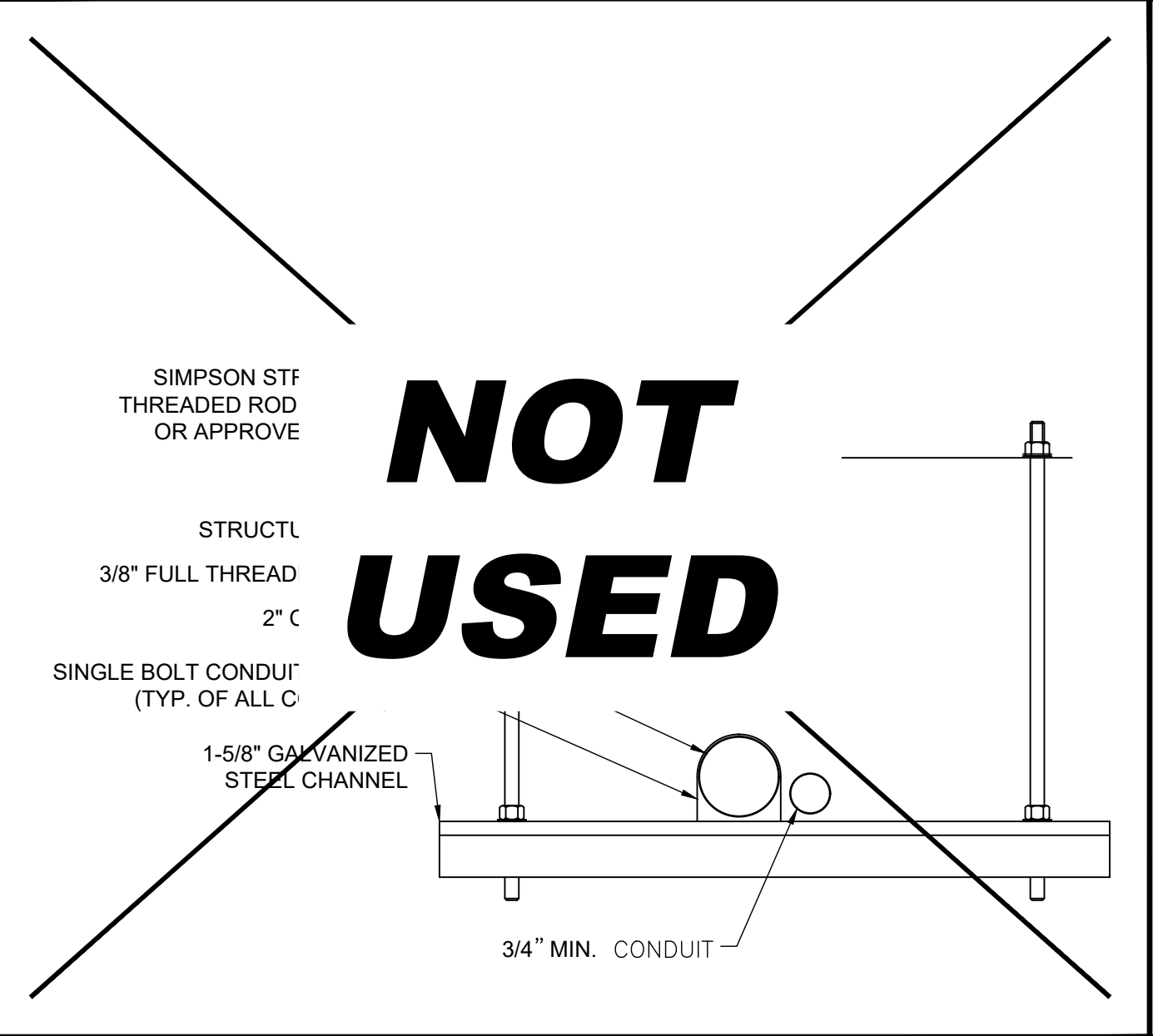
**POWER TRENCH** SCALE N.T.S. 2



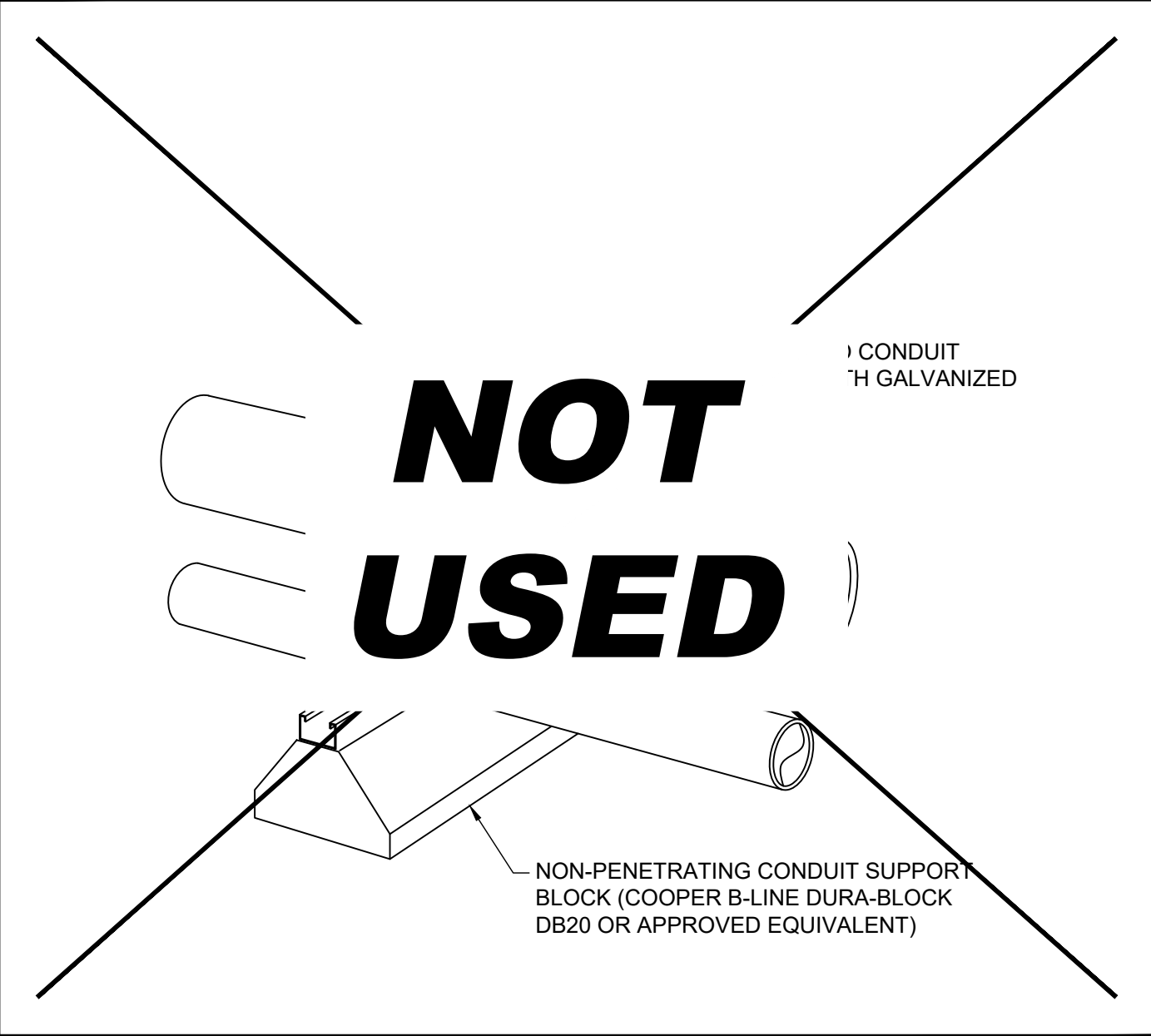
**BORE SECTION** SCALE N.T.S. 3



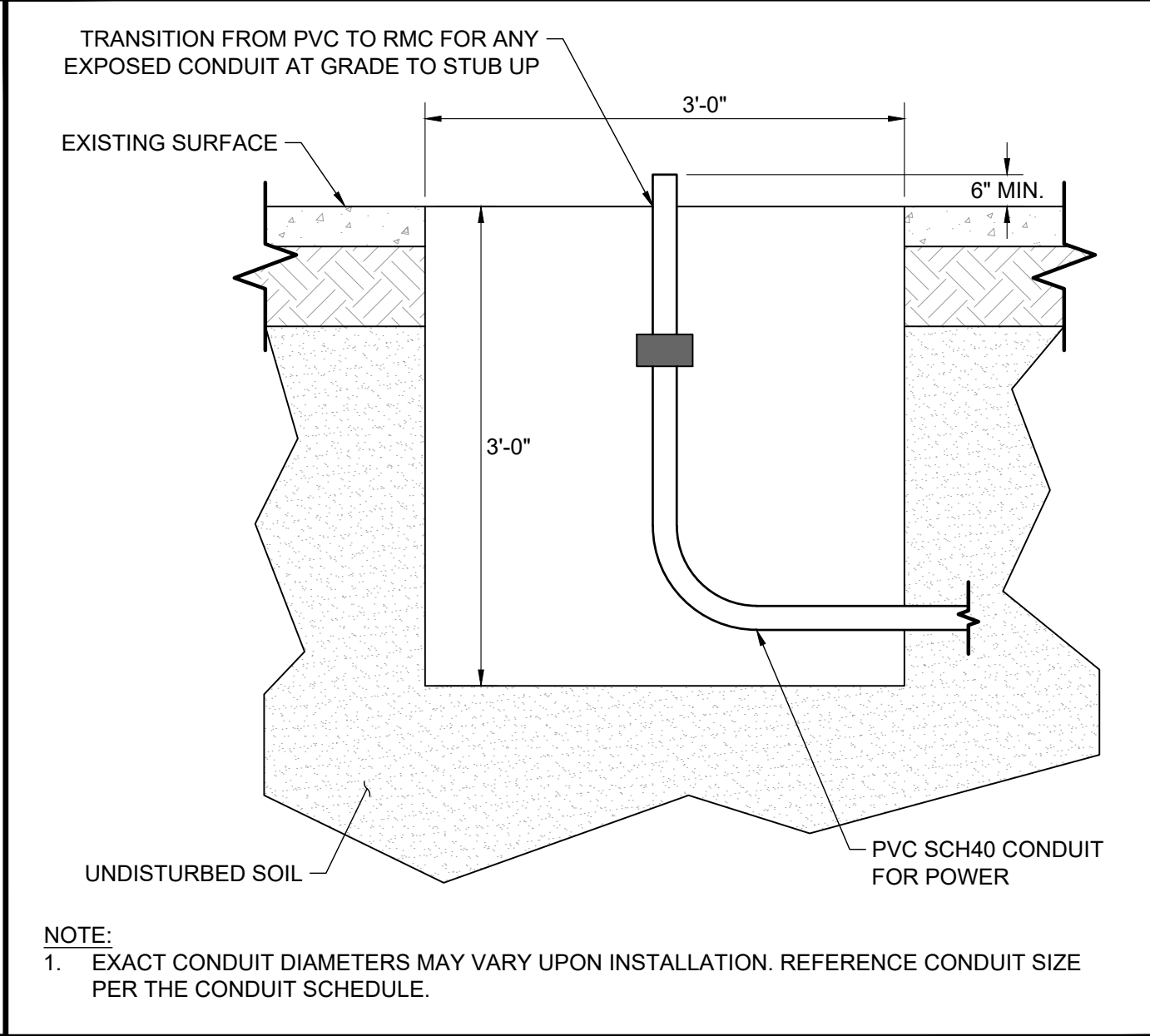
**CORE DETAIL** SCALE N.T.S. 4



**CEILING CONDUIT SUPPORT** SCALE N.T.S. 5

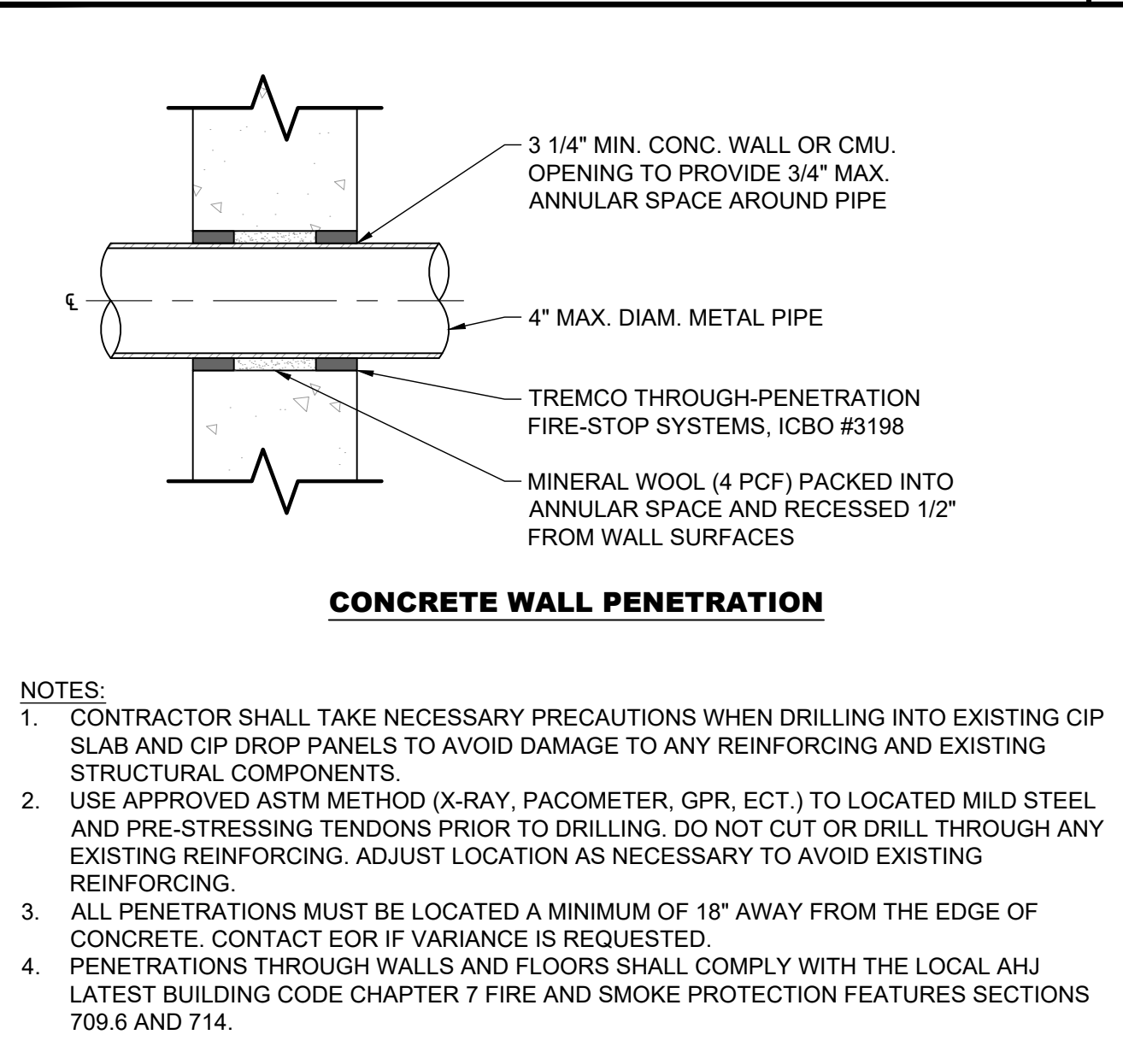


**ROOFTOP CONDUIT SUPPORT** SCALE N.T.S. 6

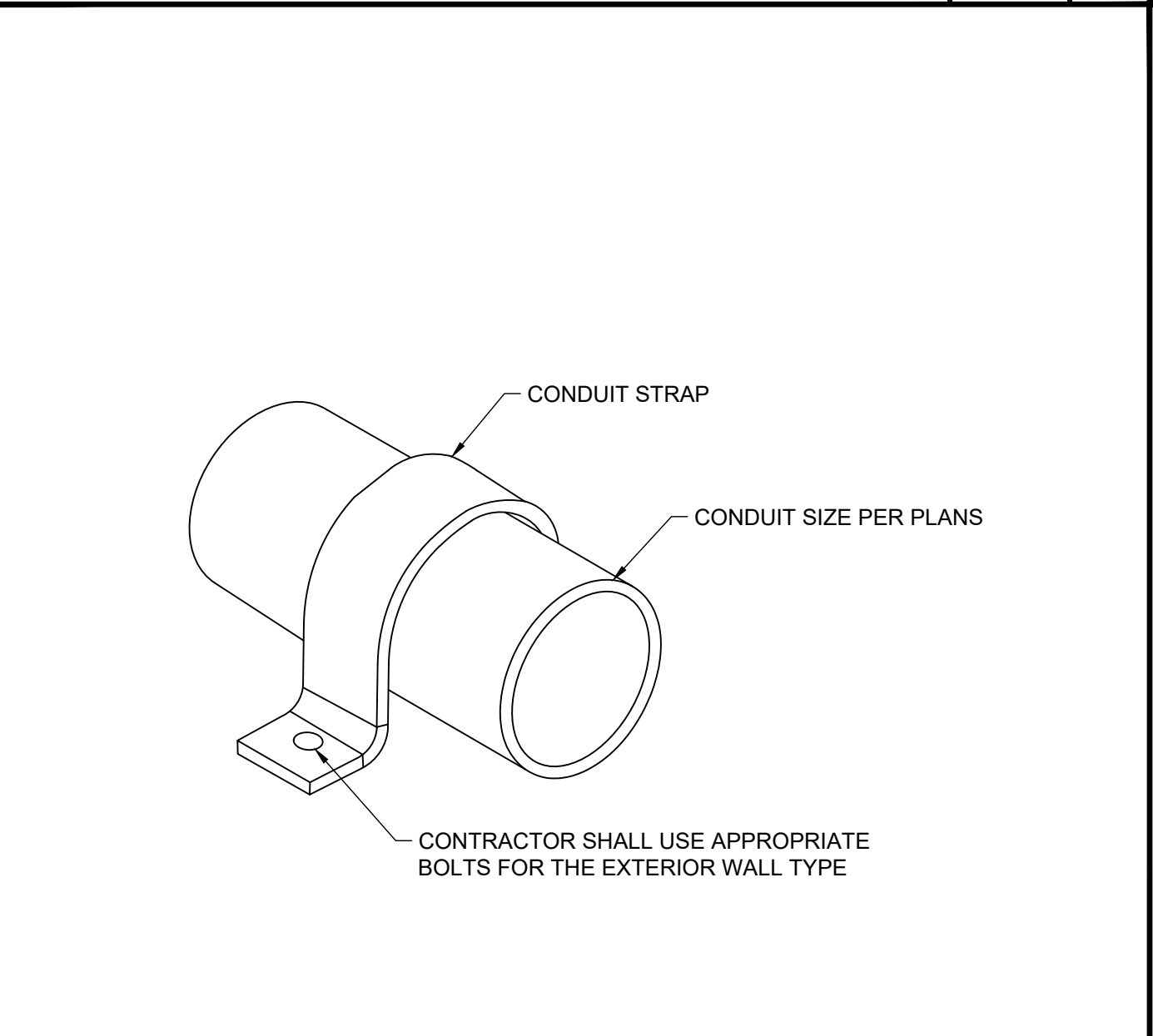


**BORE PIT** SCALE N.T.S. 7

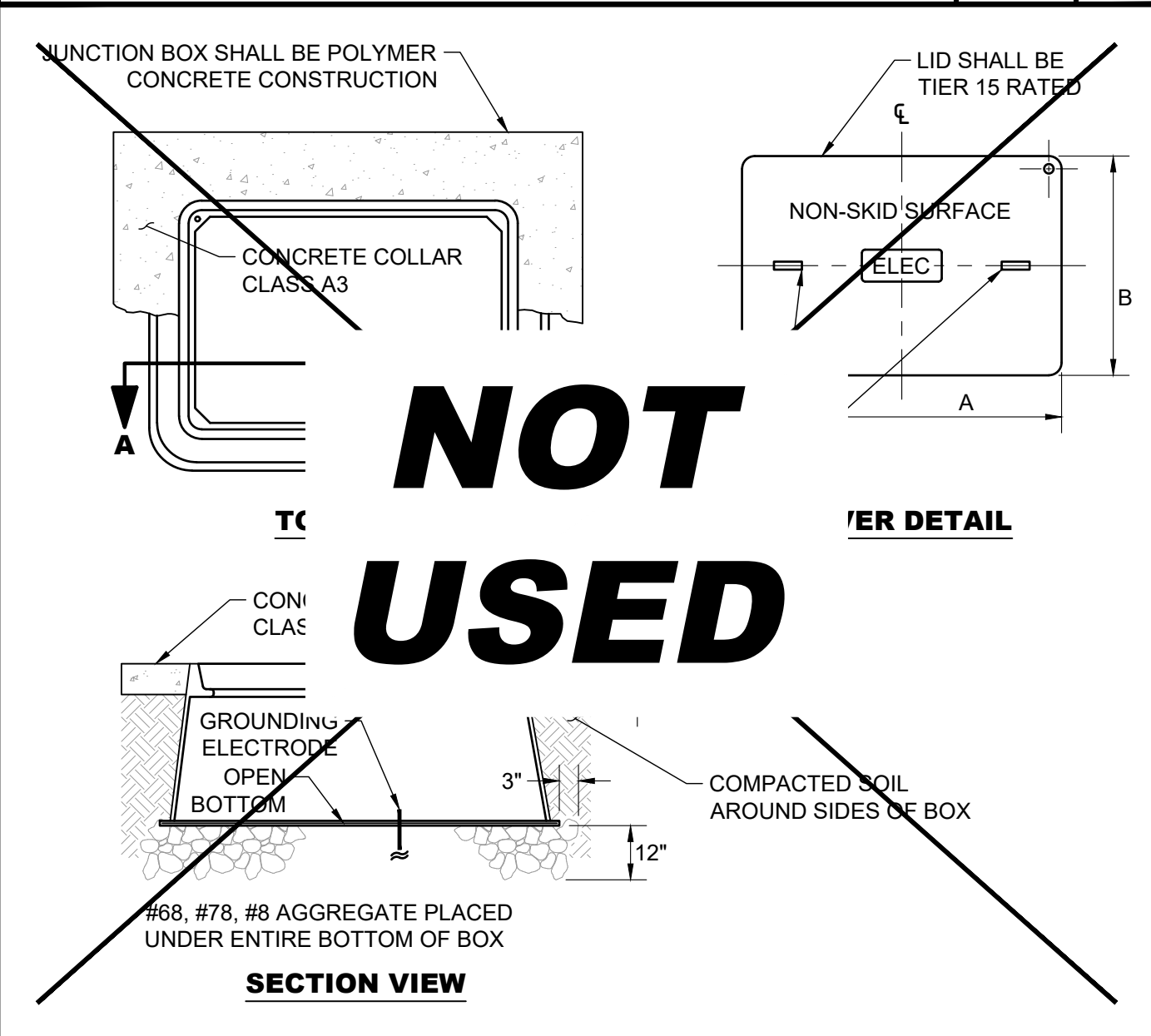
**ELECTRICAL NOTES & ABBREVIATIONS** 1



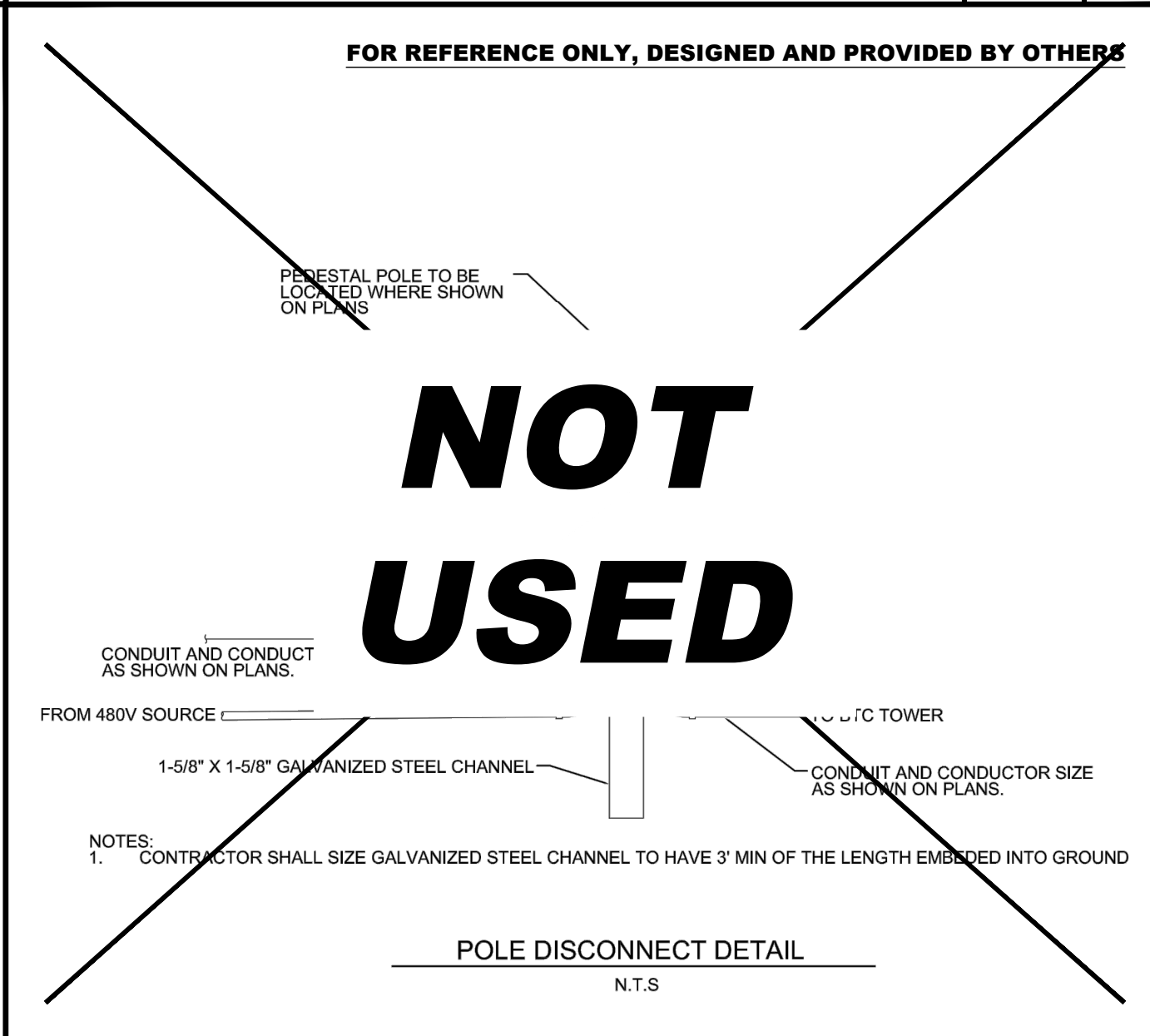
**PENETRATION DETAIL** SCALE N.T.S. 8



**WALL CONDUIT MOUNT STRAP** SCALE N.T.S. 9



**NON-TRAFFIC RATED JUNCTION BOX** SCALE N.T.S. 10



**BTC POLE DISCONNECT SWITCH** SCALE N.T.S. 11

**volta**  
155 DE HARO STREET  
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**STOP AND SHOP #683**  
180 DANBURY ROAD  
NEW MILFORD, CT 06776

SHEET TITLE  
**ELECTRICAL NOTES & DETAILS**

SHEET NUMBER  
**E2-00**

NOTE: THE ORIGINAL SIZE OF THIS PLAN IS 24" X 36". SCALE RATIO IS NOT VALID FOR REDUCED OR ENLARGED SHEET SIZES

# Proposed Amendment to the New Milford Zoning Regulations

Volta Charging, LLC, Application to Amend the New Milford Zoning Regulations, Chapter 145: Signs, Sections 145-020, 145-040, 145-050, 145-070, and the addition of Section 145-110 to permit signage and electronic displays on permitted electric vehicle charging kiosks in the business and industrial zones with frontage on Route 7 and Route 202.

Public Hearing Date: April 12, 2022

*(Proposed changes in underlined text)*

## Section 145-020 Definitions

Electric Vehicle (“EV”) Charging/Display Kiosk: A combined electric vehicle charging kiosk with internally illuminated LED displays.

Off-site sign: A sign located on a parcel of land which directs the public to a business or public attraction/location that is located on another parcel of land for the purpose of safety and convenience. An EV Charging/Display Kiosk is not included in this definition.

## Section 145-040 Prohibited Signs

The following signs are prohibited in all zones:

1. Flashing signs, with the exception of permitted EV Charging/Display Kiosks under Section 145-110.
6. Signs with electronic displays, image displays (LED, LCD etc) or moving, scrolling, and/or continuous strip lights with the exception of permitted gasoline price signs under Section 145-020[.] and permitted EV Charging/Display Kiosks under Section 145-110.

## Section 145-050 Permitted Signs

All signage described in this section requires a zoning permit.

2. Business, Industrial, Airport, and Motor Vehicle Junkyard Zones: In a B-1, B-2, B-3, B-4, I, IC, RI, Airport and MV zones, and any future non-residential zones which may be established, the following signs are permitted, as follows:
  - b. Freestanding signs, provided that only one (1) free-standing sign is allowed per lot. Freestanding sign exceptions: properties where gasoline is legally sold (not to exceed 24 square feet); properties approved to host an off-site sign in accordance with section 145-090; freestanding signs allowed as temporary under section 145-100[.]; and EV Charging/Display Kiosks allowed under section 145-110.
  - i. The maximum allowable area of a freestanding sign shall be determined as follows: the length of the lot frontage multiplied by 0.2 equals the maximum square footage allowable for the surface area of a free standing sign, up to a maximum size of forty (40) square feet in area, with the exception that in the B-4 zone, free standing signs may not exceed sixteen (16) square feet in area. Refer to sections 145-090 (Off-Site Signs) and 145-100 (Temporary Advertising Signs) for additional standards. EV Charging/Display Kiosks shall not count towards the maximum allowable freestanding sign area.



## **Section 145-070 Additional Standards**

5. Signs may advertise, identify or give publicity or notice only with respect to a use of land, buildings or structures located on the lot where the sign is located with the exception of off-site signs permitted in accordance with Section 145-090 of these regulations, EV Charging/Display Kiosks permitted in accordance with Section 145-110 of these regulations, and applicable exempt signs in accordance with Section 145-030(4), (7) and (14).

## **Section 145-110 EV Charging/Display Kiosks**

EV Charging/Display Kiosks are allowed for permitted businesses in any business or industrial zone with frontage on Route 7 and Route 202, subject to the following conditions:

1. There shall be a maximum of two (2) EV Charging/Display Kiosks per property.
2. EV Charging/Display Kiosks shall not exceed 22.5 square feet in size and 7.5 feet in height.
3. EV Charging/ Display Kiosks display screen shall not exceed 9 square feet in size.
4. EV Charging/Display Kiosks shall be equipped with an auto-dimming feature.
5. EV Charging/Display Kiosks shall limit content refresh rates to no more than every eight (8) seconds.
6. EV Charging/Display Kiosks shall be located within 100 feet of the front façade of the business building on the site.
7. EV Charging/ Display Kiosks shall be setback a minimum of 70 feet from a public right of way.



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Christopher B. Fisher  
[cfisher@cuddyfeder.com](mailto:cfisher@cuddyfeder.com)

March 30, 2022

**VIA ELECTRONIC & OVERNIGHT MAIL**

Chairman William D. Taylor  
and Members of the Zoning Commission  
Town of New Milford  
10 Main Street  
New Milford, Connecticut 06776

Re: Electric Vehicle Charging/Display Kiosks  
Zoning Text Amendment  
Route 7 EV Charging

Dear Chairman Taylor and Members of the Zoning Commission:

We are writing on behalf of Volta Charging, LLC (“Volta”) in furtherance of our appearance before the Planning Commission on March 3<sup>rd</sup> and in advance of the upcoming public hearing before your Commission on April 12<sup>th</sup>.

At its meeting earlier this month, the Planning Commission confirmed that the proposed zoning text amendments that are the subject of Volta’s petition are consistent with the Town’s POCD. A majority of members also felt that, as drafted, the text amendment protected the Town’s interests by limiting the location, number, size and design of the charging/display units at properties along the Route 7 corridor.

As part of our discussions with the Planning Commission, its members raised some excellent questions about electric vehicle (EV) charging infrastructure generally and Volta’s model of free or subsidized services to EV drivers funded by display content revenue. As such, we thought it would be helpful to your Commission to share some of our responses and provide you with additional supporting information regarding Volta and Federal and State policies and initiatives.

**Volta is America’s Biggest Supplier of EV Charging With a Growing Footprint in Connecticut**

Volta is a publicly traded company that operates the most utilized electric vehicle charging network the United States. By adopting a consumer-focused approach that offers free or subsidized charging supported by sponsored content, Volta is accelerating State and Federal objectives to support low cost and large-scale EV deployment at convenient locations. In Connecticut, Volta has already deployed EV charging/displays at many of Stop & Shop’s 88 locations in the state.

**New Milford is Part of the Federally Designated Route 7 EV Corridor**

Volta is excited about participating in the just recently announced federally designated Route 7 Alternative Fuel Corridor. This latest development has garnered enthusiastic support from many Route 7 corridor municipalities including Brookfield as noted in an article included in Attachment



A.<sup>1</sup> Indeed, communities in the corridor like Ridgefield and Norwalk have thoughtfully worked through permitting to accommodate Volta's sponsored content on digital displays, which it has installed at Stop & Shops in those communities and consistent with the company's proposal in New Milford.

As the gateway to the Berkshires and a critical corridor for motorists traveling into Litchfield County, Massachusetts and other northern destinations, New Milford is uniquely situated to provide access to zero emissions EV charging infrastructure along Route 7 for residents and visitors alike. Particularly, those areas south of the Route 7/202 split and towards the Brookfield town line where there is retail, restaurant and other commercial uses.

In fact, signage was just recently installed by CTDOT along Route 7 similar to this picture published by the agency:



Yet, the publicly available list of EV charging locations in New Milford, which is available via an "app"<sup>2</sup> maintained by state and federal agencies, identifies only a handful of charging locations in the community.

#### There is Demand for More EV Charging Locations to Meet EV Growth in Connecticut

The State is a signatory of the U.S. Department of Energy Zero-Emission Vehicle Memorandum of Understanding<sup>3</sup> with a goal of deploying 125,000-150,000 EVs on the road by 2025. Projections for the combined number of EVs currently registered in CT and predicted growth over the next 8 years are included in Attachment B. The corollary demand for publicly available universal charging infrastructure will increase dramatically in the next several years with all major automakers planning to manufacture electric models before 2030. Without such infrastructure,

<sup>1</sup> Sustainable Fairfield, *No More Anxiety!* (Nov. 2, 2021), available at: <https://sustainablefairfield.org/no-more-anxiety/>.

<sup>2</sup> U.S. Department of Energy, Alternative Fuel Data Center: Alternative Fueling Station Locator, available at: <https://afdc.energy.gov/stations#/find/nearest?fuel=ELEC&location=New%20Milford>.

<sup>3</sup> U.S. Department of Energy, Zero Emission Vehicle Deployment Support Memorandum of Understanding (CT Signatory Oct. 2013) available at: <https://afdc.energy.gov/laws/11081>.

EV growth and the corresponding reduction in emissions will be stunted locally and across the state.

#### State EV Policy Encourages Municipal Zoning Changes to Accommodate EV Charging Like Volta's

The proposed zoning text amendment in New Milford directly addresses several key focus areas in the State's EV infrastructure policies and plans because it would:

- Allow EV charging at convenient publicly accessible locations.<sup>4</sup>
- Ensure that publicly available EV infrastructure is compatible with a variety of EV models.<sup>5</sup>
- Further a "competitive approach for expanding publicly-accessible charging infrastructure, open to a variety of investment models to maximize competition and, thus, ratepayer value."<sup>6</sup>
- Amend a municipal zoning ordinance and establish permitting requirements to support EV deployment in a variety of contexts, including within existing parking areas.<sup>7</sup>

The State is encouraging municipalities to amend their zoning regulations to accommodate various forms of EV infrastructure, including consumer oriented models at retail shopping locations. Volta's private sector model of free or subsidized charging is really only possible through sponsored content integrated into digital displays on the charging kiosks. Thus, as part of the State's recognized best practices for EV charging infrastructure planning and design, there is a specific reference in its policy that EV charging stations may display "advertising and branding for the EV host or partners" as one such means of facilitating local EV adoption.<sup>8</sup>

The proposed zoning text amendments in New Milford meet all of the State's policy goals by accommodating EV charging/display kiosks in locations where public EV infrastructure is not readily available, but certainly needed along Route 7. The amendments will specifically allow the expansion of the overall EV charging network along Route 7 beyond just a couple of gas stations that have mostly Tesla branded units now.<sup>9</sup> This in turn addresses consumer confidence and reduces range anxiety through free or subsidized EV infrastructure for all types of EVs, but only with displays at conveniently located large retail stores and shopping centers in the community.

#### The Text Amendment Introduces No Adverse Impacts to the New Milford Community

To functionally accomplish Volta's model where the economic cost of EV charging can be provided as a direct benefit to EV owners at little or no contribution and without federal, state or local

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<sup>4</sup> CT DEEP EV Roadmap, p.ii-iii & 53. CT Governor's Council on Climate Change (GC3), Phase 1 Report: Near-Term Actions (Jan. 2021), p.39, available at: [https://portal.ct.gov/-/media/DEEP/climatechange/GC3/GC3\\_Phase1\\_Report\\_Jan2021.pdf](https://portal.ct.gov/-/media/DEEP/climatechange/GC3/GC3_Phase1_Report_Jan2021.pdf)

<sup>5</sup> CT DEEP EV Roadmap, p.ii-iii.

<sup>6</sup> CT DEEP EV Roadmap, p.28.

<sup>7</sup> GC3 Phase 1 Report,p.38. CT DEEP EV Roadmap, p.iv.

<sup>8</sup> CT DEEP EV Roadmap, p.60; TCI Guidelines, p.15, available at:

[https://www.transportationandclimate.org/sites/default/files/EV\\_Siting\\_and\\_Design\\_Guidelines.pdf](https://www.transportationandclimate.org/sites/default/files/EV_Siting_and_Design_Guidelines.pdf).

<sup>9</sup> "[Tesla's] publicly-available stations utilize a proprietary connector and remain inaccessible to non-Tesla drivers." CT DEEP EV Roadmap, p.ii.





taxpayer subsidies, Volta incorporates display advertising in the form of silent, static images that subtly change every 8-seconds. Volta partners with large retailers to install a limited number of these types of charging stations in parking lots where customers typically have longer visits (i.e.-grocery stores) and there is pedestrian foot traffic as well. Longer visits in turn lead to a more significant EV charge time for owners.

The “market” for this model in any locale is dependent on sponsored content where there is significant foot traffic, which for New Milford is located in large parking lots at retail locations. That is why Volta has sought to limit the text amendments to business and industrial locations along Route 7 in the Town. For the Commission’s reference, we are including GIS mapping images in Attachment C that highlight our opinion of what would be feasible under both the text amendment and might be viable properties for this type of unique business model and EV charging/display infrastructure. As identified, the relevant properties are in a specific retail section of this established business corridor, an area that is also characterized by billboards and large signage along Route 7 and sizeable retail stores and centers that are set back significant distances from the street.

We are also enclosing in Attachment D photos and photosimulations of Volta EV charging/display kiosks that are operational or proposed in other Connecticut municipalities. These photos truly show how the charging stations function with EV parking spaces, are accessory and complimentary uses at retail centers and not geared at all to off-site visibility and advertising to motorists. In fact, Volta’s displays are similar in purpose to the video advertising commonly found at gas stations, but without any of the noise or video those commercial units display when filling up at the pump.

### Conclusion

Volta looks forward to appearing before the Zoning Commission on April 12<sup>th</sup> and trusts that this supplemental information is useful in evaluating why the proposed text amendment is warranted, supported by solid policy and law, would facilitate a benefit to the community at no cost, and has support in State and Federal policies to reduce emissions by encouraging EV usage. Thank you in advance for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read 'Chris B. Fisher', with a long horizontal flourish extending to the right.

Christopher B. Fisher

### Enclosures

Cc: Laura Regan, New Milford Land Use Supervisor / Zoning Enforcement Officer  
Volta Charging, LLC  
Kimley-Horn, P.C.  
Kristen Motel, Esq.

# EXHIBIT A

# No More Anxiety!

by shawno'sullivan | Nov 2, 2021 | Learn About EVs | 0 comments



By Shawn O'Sullivan

Connecticut is proving that you *can* get there from here... in an EV. On Oct. 27, Live Green dropped a new pin on CT's EV Roadmap: the designation of Route 7 as an EV Corridor, with a celebration and ribbon cutting held at Fodor Farm in Norwalk. On hand were state, legislative and municipal officials, all cheerleading for this road to the future- each speaker adding a unique view of the journey.

An Alternative Fuel Corridor is a federally designated highway with EV chargers located no more than 50 miles apart, and no more than 5 miles off the highway. CT now boasts 5 of them, I 84, I 91, I 95, I 395, and



Route 7. Daphne Dixon, Executive Director and Co Founder of Live Green, host of the event, noted that signage at the beginning and end of the highway showing that alternative fuel options are available, “range anxiety,” formerly a deterrent to buying an EV, is diminished. Thanks to this growing infrastructure, worry no more.

Carlo Leone, Senior Advisor at Connecticut’s Department of Transportation (CTDOT) noted CT’s commitment to supporting statewide electrification and the Federal Highway Administration’s Alternate Fuel Program. “Route 7 is a route of national importance. It goes all the way up through Massachusetts and Vermont. It’s going to be transitional and have a huge impact. It’s a vital tool to improve awareness and to ease that transition as we move away from fossil fuel vehicles.”



Carlo Leone and Daphne Dixon during Live Green’s celebration of Route 7s designation as an EV corridor. Fodor Farm, Norwalk

Norwalk’s Mayor Rilling, concurred that EVs are the wave of the future; he also noted that Mother Earth provides what we need, and we shouldn’t be polluting her. “By everybody working together we can

reduce carbon emissions and repair the atmosphere so that we don't have climate change. So let's all commit to making sure this happens."

The sponsor for the event was Juice Bar EV, a Norwalk CT company. Their Senior VP of Sales, Paul Young, is enthusiastic about their mission. "Right now it's truly about educating people about the electric cars and support thereof, and also, as we like to say 'planting flags,' and the Route 7 corridor is a perfect example of planting flags. Those flags out there - waving along the highway or the Route 7 corridor tell people 'Hey you can charge here! It's ok! You can get an electric vehicle!" He also echoed Mr Leone's sentiment, "Let's go all the way up to Burlington; let's continue this corridor. Young also added another nice note to the very positive day: that Juice Bar EV would be donating a charging station to Fodor Farm.

First Selectman Steven Dunn, of Brookfield, CT is bullish on EV readiness, and noted that his town is championing this by starting from the bottom up: completely revamping their zoning laws and getting developers on board. "What a time to make sure that every property that comes in has zoning that requires them to make it EV ready."



Jim Marpe, First Selectman of Westport, speaks during Live Green's Ribbon cutting event at Fodor Farm in Norwalk celebrating the opening of the Route 7 Ev Corridor

Westport is leading by example. First Selectman Jim Marpe noted that Westport has the distinction more EVs per capita than any other town in CT. He said the town was working to ensure that EV chargers were considered in their municipal budgeting, and that they were aiming for 30% of their fleet to be electric by 2025.

Barry Kresch, EV Club of CT spoke about the important role the club has played in CT's EV readiness, from advocating for legislature on EV's to educating the public through events and their website.

Wilton Go Green's Chair, Tammy Thornton reminded those present about the important equity aspect of CT's EV Roadmap, with a focus on those that can't afford EVs: "All residents across the state, and not just those along the corridor, should have access to electric vehicle readiness through public transportation and school transportation, knowing that their air can be clean even if they can't afford an electric vehicle."

Senator Bob Duff brought a note of optimism. "Sometimes I think when we hear such bad news between the big storms and climate change and all the other things that go on, people get overwhelmed and not really sure how they can make an impact. A lot of times it's not just the big things that people can do. It's the step by step things that help over time improve our environment and improve our world." Adding a bit of brevity to explain the corridor he said, "Yes you can purchase these vehicles, and you will not run out somewhere along Route 7 and never be heard from again, and that you will actually get from point A to point B and back to point A again." Yes, he got the laugh.







Senators Bob Duff and Will Haskell talk during Live Green's Ribbon cutting event at Fodor Farm in Norwalk, celebrating the opening of the Route 7 Ev Corridor.

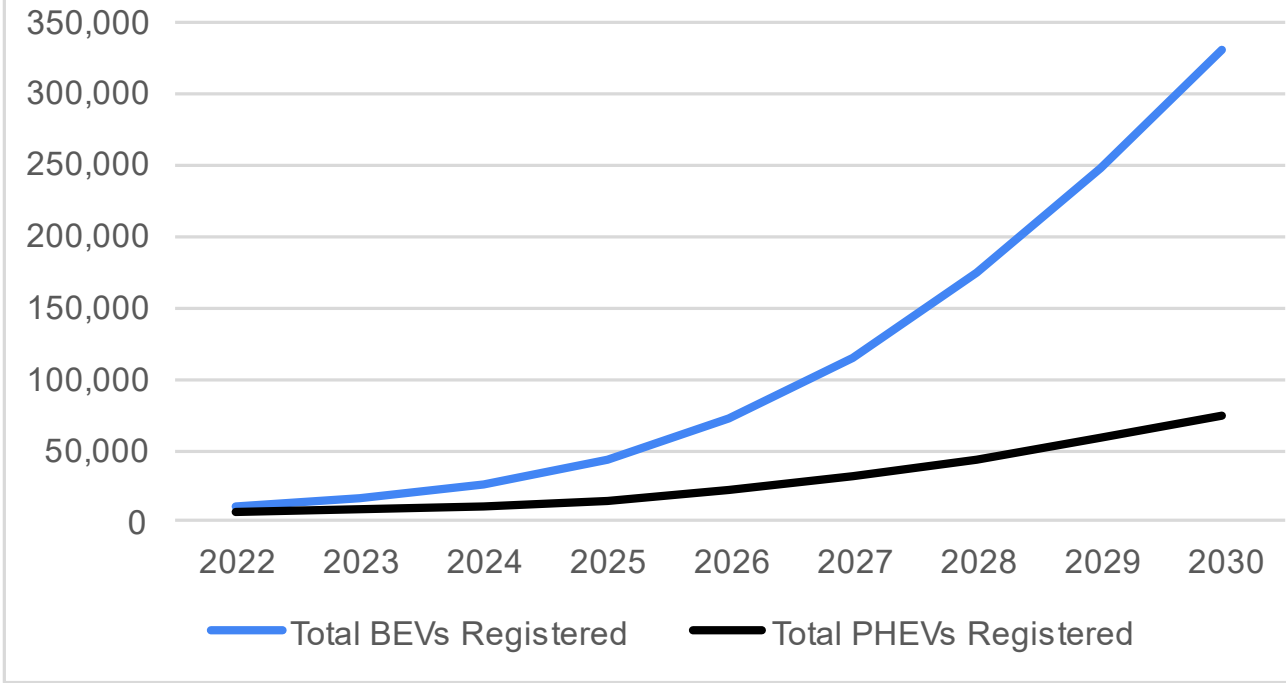
On a more sobering note Senator Will Haskell, Chair of the CT Legislature's Transportation Committee, noted, "We know that 40% of carbon emissions come from the transportation sector. If we are going to get serious about meeting our environmental goals we have to hone in on transportation and transportation related emissions. These emissions especially negatively impact urban communities and communities of color ; they impact those who rely on the bus to get to and from work every day; they impact children who breathe in diesel fumes as they go to school every day." He went on, "This designation means finally that state and local governments are working together to give our communities the tools to actually achieve these ambitious goals."

As Selectman Marpe said earlier, "We're really talking about saving the planet here, one step at a time." Maybe that should read "one charge at a time."



# **EXHIBIT B**

## Predicted EV Growth in CT



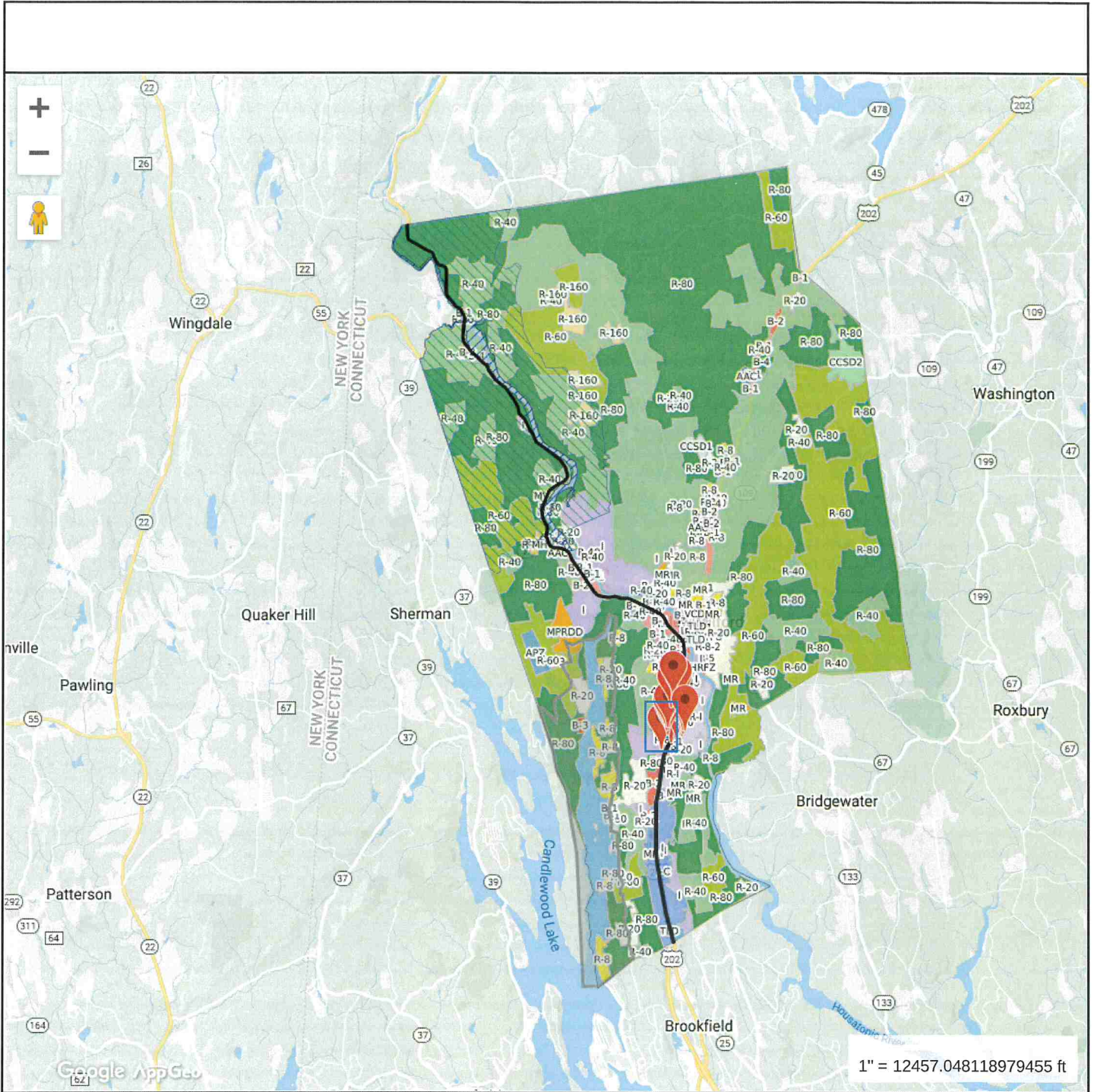
	2022	2023	2024	2025	2026	2027	2028	2029	2030
Total BEVs Registered <sup>1</sup>	10,279	15,360	25,339	43,243	71,937	114,978	174,514	248,574	330,824
Total PHEVs Registered <sup>2</sup>	6,996	8,337	10,863	15,175	21,718	30,989	43,188	57,948	74,445
Total number of cars requiring EV infrastructure	17,275	23,697	36,202	58,418	93,655	145,967	217,701	306,523	405,269

<sup>1</sup> BEV stands for “Battery Electric Vehicle” which means the vehicle has no internal combustion engine, fuel tank or exhaust pipe (<https://www.aptiv.com/en/insights/article/bev-phev-or-hev-the-differences-affect-the-architecture>)

<sup>2</sup> PHEV stands for “Plug-in Hybrid Electric Vehicle” which have both an electric motor recharged from an external plug and an internal combustion engine with a fuel tank (<https://www.aptiv.com/en/insights/article/bev-phev-or-hev-the-differences-affect-the-architecture>)

# EXHIBIT C





**Property Information**  
 Property ID 18.1/29  
 Location 164 DANBURY RD  
 Owner U B LITCHFIELD LLC



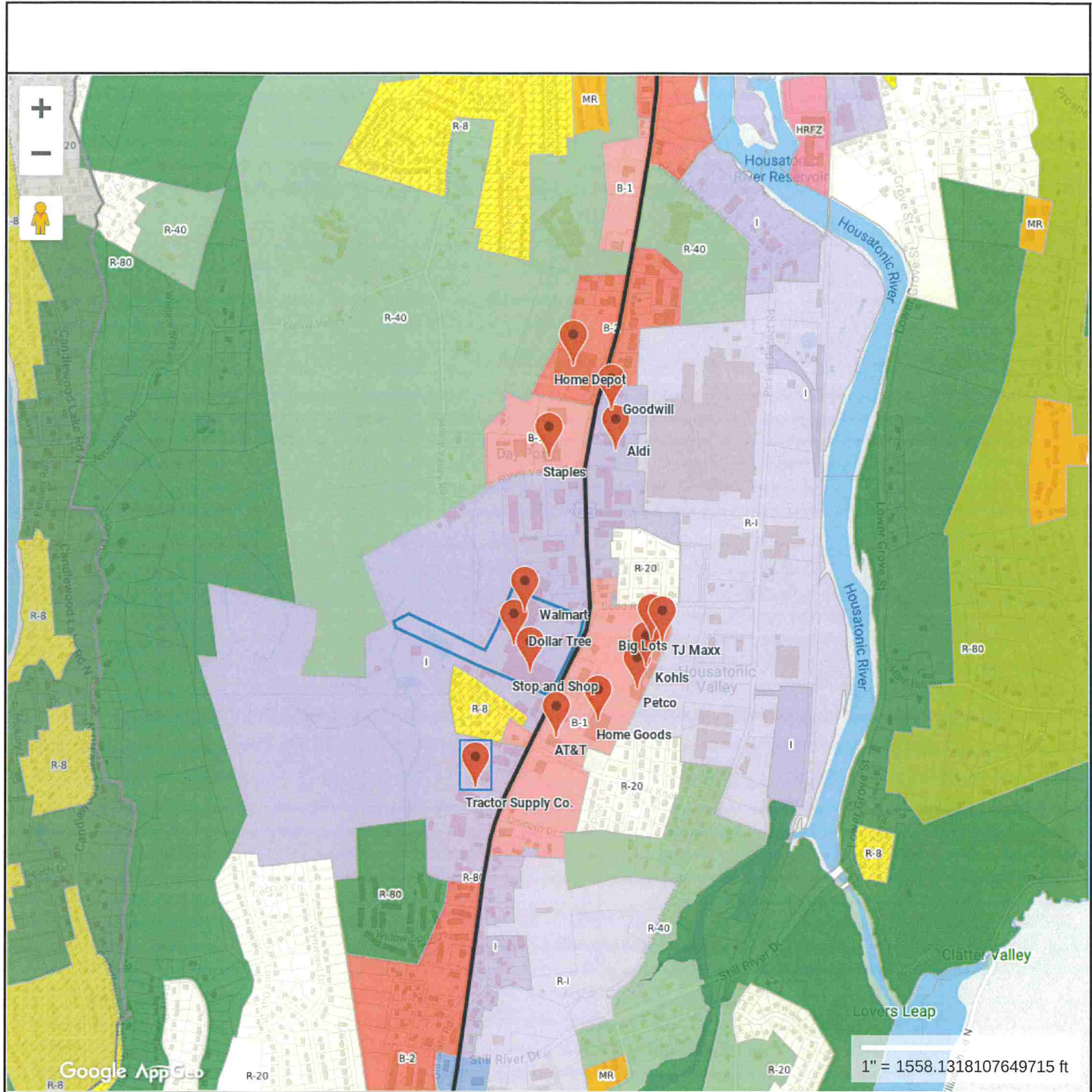
**MAP FOR REFERENCE ONLY  
 NOT A LEGAL DOCUMENT**

Town of New Milford, CT makes no claims and no warranties, expressed or implied, concerning the validity or accuracy of the GIS data presented on this map.

Geometry updated 12/22/2021  
 Data updated daily

Print map scale is approximate. Critical layout or measurement activities should not be done using this resource.





**Property Information**

Property ID 18.1/29  
 Location 164 DANBURY RD  
 Owner U B LITCHFIELD LLC



**MAP FOR REFERENCE ONLY  
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Town of New Milford, CT makes no claims and no warranties, expressed or implied, concerning the validity or accuracy of the GIS data presented on this map.

Geometry updated 12/22/2021  
 Data updated daily

Print map scale is approximate. Critical layout or measurement activities should not be done using this resource.

# Map Theme Legends

## Zoning

-  R-20 RESIDENTIAL ZONE
-  R-40 RESIDENTIAL ZONE
-  R-60 RESIDENTIAL ZONE
-  R-80 RESIDENTIAL ZONE
-  R-8 RESIDENTIAL ZONE
-  R-5 RESIDENTIAL ZONE
-  R-8-2 RESIDENTIAL TWO-FAMILY ZONE
-  R-160 RESIDENTIAL ZONE
-  R-MH MOBILE HOME DISTRICT
-  AAC ACTIVE ADULT COMMUNITY ZONE
-  MR MULTIPLE-RESIDENCE DISTRICT
-  MPRDD MAJOR PLANNED RESIDENTIAL DEVELOPMENT DISTRICT
-  RI RESTRICTED INDUSTRIAL
-  I INDUSTRIAL ZONE
-  I-C INDUSTRIAL/COMMERCIAL ZONE
-  B-1 RESTRICTED BUSINESS ZONE
-  B-2 GENERAL BUSINESS ZONE
-  B-3 LAKE BUSINESS ZONE
-  B-4 BUSINESS ZONE
-  VCD VILLAGE CENTER DISTRICT
-  MV MOTOR VEHICLE JUNKYARD DISTRICT
  
-  CCSD1 CLUSTER CONSERVATION SUBDIVISION
-  DISTRICT #1
-  CCSD2 CLUSTER CONSERVATION SUBDIVISION
-  DISTRICT #2
  
-  APZ AIRPORT DISTRICT
-  TLD TOWN LANDMARK DISTRICT
-  HRFZ HOUSATONIC RIVERFRONT ZONE
  
-  CANDLEWOOD LAKE WATERSHED DISTRICT
  
-  HOUSATONIC RIVER OVERLAY ZONE
-  INNER ZONE
-  OUTER ZONE

# EXHIBIT D



Volta Charging, LLC

**Photos of Existing Electric Vehicle Charging/Display Kiosks**



Stop & Shop- 505 North Main Street, Southington, CT

**Volta Charging, LLC**

**Photos of Existing Electric Vehicle Charging/Display Kiosks**



Black Rock Shopping Center - 2181 Black Rock Turnpike, Fairfield, CT



**Volta Charging, LLC**

**Photos of Existing Electric Vehicle Charging/Display Kiosks**



Stop & Shop - 597 Farmington Avenue, Bristol, CT



# Stop & Shop Litchfield

55 Village Green Dr, Litchfield, CT 06759

















# EXHIBIT E

ZONING COMMISSION: TOWN OF NEW MILFORD  
COUNTY OF LITCHFIELD: STATE OF CONNECTICUT

-----X  
In the Matter of the Application of

**Volta Charging, LLC**

**PETITION**

Petition to the Amend the Zoning Regulations of the Town of New Milford to Permit Electric Vehicle Charging/Display Kiosks on Parcels in Business and Industrial Zones that Will Apply to Real Property Located at 180 Danbury Road, Designated on the Tax Map of the Town of New Milford as Section 18, Block 1, Lot 29 (also known as “164 Danbury Road”)

-----X

TO THE HONORABLE CHAIRMAN AND MEMBERS OF THE ZONING COMMISSION OF THE TOWN OF NEW MILFORD

The Petition of Volta Charging, LLC (“Volta” or the “Petitioner”) respectfully shows and alleges in support of Petitioner’s request:

1. PETITIONER: Volta Charging, LLC is a corporation organized and existing under the laws of the State of Delaware and is requesting text amendments to the Zoning Regulations of the Town of New Milford, Connecticut to permit EV Charging/Display Kiosks on parcels classified in Commercial and Industrial Districts subject to specific bulk, design and visibility requirements.

Volta operates electric vehicle (“EV”) charging networks throughout the United States and provides free charging in convenient locations. By generating revenue through sponsored content, Volta creates value and passes that on to EV owners who can charge while they shop. This encourages consumer adoption of electric vehicles with net reductions in carbon emissions.

Volta is partnering with Stop & Shops across the Northeast to establish on-site electric vehicle charging kiosks that enhance the shift from combustion-powered miles to electric miles. The charging kiosks include a digital display feature for various sponsored content, which enables Volta to provide the vehicle charge at no cost to the driver while it is parked on site.

2. THE PARCELS & FUTURE PROPOSAL: The Petitioner is proposing to install two (2) EV Charging/Display Kiosks in the parking lot of the Stop & Shop located at 180 Danbury Road (also known as 164 Danbury Road) (the “Parcel”) in the Town of New Milford, Connecticut.

The Parcel is currently classified within the Industrial (“I”) Zoning District and is situated in the New Milford Shopping Plaza, which tenants include numerous retail stores, including a Stop & Shop and a Walmart, personal service shops, restaurants and a gas station.

Volta proposes to convert two (2) standard parking spaces into two (2) electric vehicle spaces with charging kiosks located in existing landscaped islands. The kiosks are

approximately seven (7) feet tall and three (3) feet wide and include digital displays that are four (4) feet tall and 2¼ feet wide.

The EV Charging/Display Kiosks will be located in the first row of parking spaces, closest to the Stop & Shop building and its entrance to target pedestrian traffic and are not intended for any off-site visibility. The digital displays themselves have an auto-dimming feature and are equipped to display subtle motion with sponsored content changing at a maximum of 8-second intervals. Concept drawings and renderings of the proposed EV Charging/Display Kiosks are annexed hereto as **Exhibit D**.

3. IN FURTHERANCE OF THE TOWN OF NEW MILFORD'S PLAN OF CONSERVATION AND DEVELOPMENT, THE PROPOSED EV CHARGING/DISPLAY KIOSKS WILL EXPAND TRANSPORTATION INFRASTRUCTURE, PROMOTE SUSTAINABILITY AND PROVIDE CONVENIENT AMENITIES: The proposed zoning amendments will permit EV Charging/Display Kiosks in limited locations to serve and complement the business and industrial zones while also enhancing the Route 7 South/Route 202 corridor. See **Exhibit C** – Zoning Text Amendments.

The Town of New Milford 2021 Plan of Conservation and Development (“POCD”) encourages the expansion of transportation infrastructure for all users<sup>1</sup> and promotes incorporating sustainability into Town regulations and development policies.<sup>2</sup> The POCD specifically identifies the task of creating “consistent infrastructure for sustainability.”<sup>3</sup> In fact, the POCD notes that “[a]dequate electrical power is expected to be available to meet the needs of local users and accommodate new technologies and uses such as electric vehicles and electric charging stations.”<sup>4</sup>

Among the POCD’s goals is to advocate for an ongoing certification in Sustainable Connecticut and create consistent infrastructure for sustainability.<sup>5</sup> As a silver-certified member of Sustainable Connecticut, New Milford strives to implement best practice “actions”, one of which is supporting zero emission vehicles deployment by creating EV infrastructure for residents and travelers.<sup>6</sup>

In encouraging New Milford to continue its sustainability efforts<sup>7</sup>, the POCD recognizes that modifications to the current zoning regulations are necessary to attract new investments<sup>8</sup> and that land use policies and regulations should reflect the type of business development envisioned for the area.<sup>9</sup> The POCD also highlights the need to improve business and industrial locations along the Route 7 South/Route 202 corridor, support development in all business and industrial zones and capitalize on business opportunities.<sup>10</sup>

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<sup>1</sup> See POCD, pg. 6.

<sup>2</sup> See POCD, pg. 139.

<sup>3</sup> See POCD, pg. 139.

<sup>4</sup> See POCD, pg. 103 (emphasis added).

<sup>5</sup> See POCD, pg. 139.

<sup>6</sup> See Sustainable Connecticut, Action 6.4 – Support Zero Emission Vehicle Deployment, <https://sustainablect.org/actions-certifications/actions/#open/action/42>.

<sup>7</sup> See POCD, pg. 122.

<sup>8</sup> See POCD, pg. 139.

<sup>9</sup> See POCD, pg. 140.

<sup>10</sup> See POCD, pg. 134.



The requested zoning text amendments are integral to encouraging sustainable infrastructure options for New Milford's citizens and visitors that also promote business development. Volta's uniquely situated EV Charging Kiosks encourage the use of electric vehicles by conveniently providing free charging stations to Stop & Shop customers. Permitting the display component of the Kiosks will enhance local economic development by enticing resident EV drivers to shop locally and enjoy free vehicle charging.

Further, this text amendment is timely given recent federal and state initiatives to encourage EV deployment and meet the future infrastructure needs of New Milford.<sup>11</sup> The proposed amendments will accommodate much needed no-cost electric vehicle infrastructure that is easily accessible from a heavily traveled State Route.

The proposed zoning amendments include criterion specifically designed to ensure that EV Charging/Display Kiosks will be compatible with surrounding land uses in this commercial district along Danbury Road. Amending the Zoning Regulations to permit EV Charging/Display Kiosks in business and industrial zones subject to specific location, bulk, design and visibility requirements will further New Milford's goals of enhancing sustainable transportation infrastructure, improving industrial and business zones, and adopting regulations that promote sustainability and attract new investment.

WHEREFORE, the Petitioner respectfully requests that this Petition be granted in its entirety thereby amending the Zoning Regulations of the Town of New Milford to permit EV Charging/Display Kiosks in business and industrial zones on parcels fronting on Route 7 / Route 202, subject to specific location, bulk, design and visibility requirements.

Dated: January 31, 2022

Respectfully submitted,



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Kristen Motel  
**Cuddy & Feder LLP**  
Attorneys for Petitioners  
445 Hamilton Avenue, 14<sup>th</sup> Floor  
White Plains, New York 10601

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<sup>11</sup> On August 5, 2021, the President issued an Executive Order establishing a goal for 50% of all vehicles sold in 2030 to be zero emission vehicles. [Executive Order on Strengthening American Leadership in Clean Cars and Trucks](#). The Connecticut Governor's Council on Climate Change (GC3) released a report in January 2021, [Taking Action on Climate Change and Building a More Resilient Connecticut for All](#), which includes the following recommendation for municipalities, in relevant part: "Expand electric vehicle (EV) charging network to ensure consumer confidence and reduce range anxiety." Pg. 38. As revealed by the State's [2021 Greenhouse Gas Inventory](#), Connecticut is currently not on track to meet its 2030 Global Warming Solutions Act (GWSA) emission target, due in part to substandard EV adoption rates. Indeed, "approximately 17,217 passenger EVs are registered in Connecticut – a small fraction of the 500,000 electric light-duty vehicles that the GC3 has projected the state would need in order to meet the 2030 GWSA targets. [2021 Greenhouse Gas Inventory](#), pg. 4.